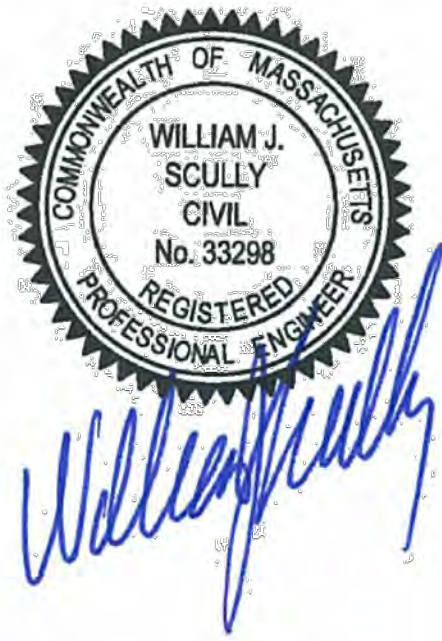




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SHEET NUMBER		1	
COMPLETE STREETS PROJECTS PREPARED FOR TOWN OF HOLLISTON		MASSACHUSETTS HOLLISTON	
COVER SHEET		KHA PROJECT 112613001 DATE 09/29/2023 SCALE AS SHOWN DESIGNED BY WJS DRAWN BY WJS CHECKED BY WJS	
		 <p>           © 2023 KIMLEY-HORN AND ASSOCIATES, INC.            404 WYMAN STREET, SUITE 385, WALTHAM, MA 02451            PHONE: 781-328-0876            WWW.KIMLEY-HORN.COM         </p>	
		No.	REVISIONS
		DATE	BY



Plotted By:Graham, Payton Sheet Set:Kha Layout:LEGEND September 28, 2023 03:31:44pm K:\BOS-TP101\12613001\_Holliston\_Tier 3 Eng-Const\CAD\PlanSheets\C2.0 LEGEND.dwg

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ABBREVIATIONS

GENERAL	
AADT	ANNUAL AVERAGE DAILY TRAFFIC
ABAN	ABANDON
ADJ	ADJUST
APPROX.	APPROXIMATE
A.C.	ASPHALT CONCRETE
ACCM PIPE	ASPHALT COATED CORRUGATED METAL PIPE
BERM	CAP COD BERM
BIT.	BITUMINOUS
BC	BOTTOM OF CURB
BD.	BOUND
BL	BASELINE
BLDG	BUILDING
BM	BENCHMARK
BO	BY OTHERS
BOS	BOTTOM OF SLOPE
BR.	BRIDGE
CB	CATCH BASIN
CBCI	CATCH BASIN WITH CURB INLET
CC	CEMENT CONCRETE
CCM	CEMENT CONCRETE MASONRY
CEM	CEMENT
CI	CURB INLET
CIP	CAST IRON PIPE
CLF	CHAIN LINK FENCE
CL	CENTERLINE
CMP	CORRUGATED METAL PIPE
CSP	CORRUGATED STEEL PIPE
CO.	COUNTY
CONC	CONCRETE
CONT	CONTINUOUS
CONST	CONSTRUCTION
CR GR	CROWN GRADE
DHV	DESIGN HOURLY VOLUME
DI	DROP INLET
DIA	DIAMETER
DIP	DUCTILE IRON PIPE
DW	STEADY DON'T WALK - PORTLAND ORANGE
DWY	DRIVEWAY
EHH	ELECTRIC HANDHOLE
ELEV (or EL.)	ELEVATION
EMB	EMBANKMENT
EMH	ELECTRIC MANHOLE
EOP	EDGE OF PAVEMENT
ET	ELECTRIC TRANSFORMER
EXIST (or EX)	EXISTING
EXC	EXCAVATION
F&C	FRAME AND COVER
F&G	FRAME AND GRATE
FDN.	FOUNDATION
FLDSTN	FIELDSTONE
GAR	GARAGE
GD	GROUND
GG	GAS GATE
GI	GUTTER INLET
GIP	GALVANIZED IRON PIPE
GRAN	GRANITE
GRAV	GRAVEL
GRD	GUARD
GW	GUY WIRE
HDW	HEADWALL
HMA	HOT MIX ASPHALT
HOR	HORIZONTAL
HYD	HYDRANT
INV	INVERT
JCT	JUNCTION
L	LENGTH OF CURVE
LB	LEACH BASIN
LP	LIGHT POLE
LT	LEFT
MAX	MAXIMUM
MB	MAILBOX
MH	MANHOLE
MHB	MASSACHUSETTS HIGHWAY BOUND
MIN	MINIMUM
NIC	NOT IN CONTRACT
NO.	NUMBER
OH	OVERHEAD WIRE
PC	POINT OF CURVATURE
PCC	POINT OF COMPOUND CURVATURE
P.G.L.	PROFILE GRADE LINE


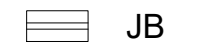





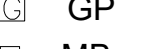

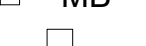









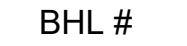



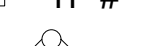



























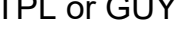















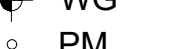

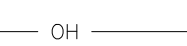

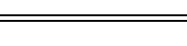





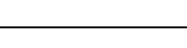

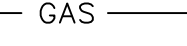

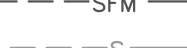

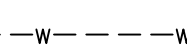







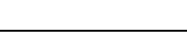

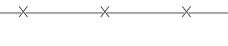

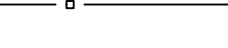



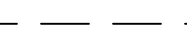

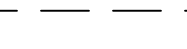









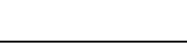

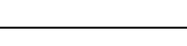

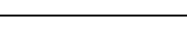



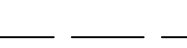


















ABBREVIATIONS (cont.)

GENERAL	
PI	POINT OF INTERSECTION
POC	POINT ON CURVE
POT	POINT ON TANGENT
PRC	POINT OF REVERSE CURVATURE
PROJ	PROJECT
PROP	PROPOSED
PSB	PLANTABLE SOIL BORROW
PT	POINT OF TANGENCY
PVC	POINT OF VERTICAL CURVATURE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENCY
PVMT	PAVEMENT
PWW	PAVED WATER WAY
R	RADIUS OF CURVATURE
R&D	REMOVE AND DISPOSE
RCP	REINFORCED CONCRETE PIPE
RD	ROAD
RDWY	ROADWAY
REM	REMOVE
RET	RETAIN
RET WALL	RETAINING WALL
ROW	RIGHT OF WAY
RR	RAILROAD
R&R	REMOVE AND RESET
R&S	REMOVE AND STACK
RT	RIGHT
SB	STONEBOUND
SHLD	SHOULDER
SMH	SEWER MANHOLE
ST	STREET
STA	STATION
SSD	STOPPING SIGHT DISTANCE
SHLO	STATE HIGHWAY LAYOUT LINE
SW	SIDEWALK
T	TANGENT DISTANCE OF CURVE/TRUCK %
TAN	TANGENT
TEMP	TEMPORARY
TC	TOP OF CURB
TMH	TELEPHONE MANHOLE
TOS	TOP OF SLOPE
TYP	TYPICAL
UP	UTILITY POLE
VAR	VARIES
VERT	VERTICAL
VC	VERTICAL CURVE
VGC	VERTICAL GRANITE CURBING
WCR	WHEEL CHAIR RAMP
WG	WATER GATE
WIP	WROUGHT IRON PIPE
WM	WATER METER/WATER MAIN
X-SECT	CROSS SECTION




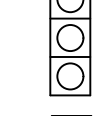

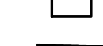





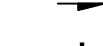












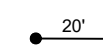




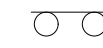

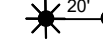
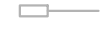
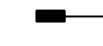









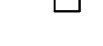

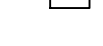

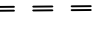


TRAFFIC SIGNAL ABBREVIATIONS

CAB	CABINET
CCVE	CLOSED CIRCUIT VIDEO EQUIPMENT
DW	STEADY UPRAISED HAND
FDW	FLASHING UPRAISED HAND
FR	FLASHING CIRCULAR RED
FRL	FLASHING RED LEFT ARROW
FRR	FLASHING RED RIGHT ARROW
FY	FLASHING CIRCULAR YELLOW
FYL	FLASHING YELLOW LEFT ARROW
FYR	FLASHING YELLOW RIGHT ARROW
G	STEADY CIRCULAR GREEN
GL	STEADY GREEN LEFT ARROW
GR	STEADY GREEN RIGHT ARROW
GSL	STEADY GREEN SLASH LEFT ARROW
GSR	STEADY GREEN SLASH RIGHT ARROW
GV	STEADY GREEN VERTICAL ARROW
OL	OVERLAP
PED	PEDESTRIAN
PTZ	PAN, TILT, ZOOM
R	STEADY CIRCULAR RED
RL	STEADY RED LEFT ARROW
RR	STEADY RED RIGHT ARROW
TR SIG	TRAFFIC SIGNAL
TSC	TRAFFIC SIGNAL CONDUIT
W	STEADY WALKING PERSON
Y	STEADY CIRCULAR YELLOW
YL	STEADY YELLOW LEFT ARROW

























GENERAL SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
		JERSEY BARRIER
		CATCH BASIN
		CATCH BASIN CURB INLET
		FLAG POLE
		GAS PUMP
		MAIL BOX
		POST SQUARE
		POST CIRCULAR
		WELL
		ELECTRIC HANDHOLE
		FENCE GATE POST
		GAS GATE
		BORING HOLE
		MONITORING WELL
		TEST PIT
		HYDRANT
		LIGHT POLE
		COUNTY BOUND
		GPS POINT
		CABLE MANHOLE
		DRAINAGE MANHOLE
		ELECTRIC MANHOLE
		GAS MANHOLE
		MISC MANHOLE
		SEWER MANHOLE
		TELEPHONE MANHOLE
		WATER MANHOLE
		MASSACHUSETTS HIGHWAY BOUND
		MONUMENT
		STONE BOUND
		TOWN OR CITY BOUND
		TRAVERSE OR TRIANGULATION STATION
		TROLLEY POLE OR GUY POLE
		TRANSMISSION POLE
		UTILITY POLE W/ FIREBOX
		UTILITY POLE WITH DOUBLE LIGHT
		UTILITY POLE W / 1 LIGHT
		UTILITY POLE
		BUSH
		TREE
		STUMP
		SWAMP / MARSH
		WATER GATE
		PARKING METER
		OVERHEAD CABLE/WIRE
		CURBING
		CONTOURS (ON-THE-GROUND SURVEY DATA)
		CONTOURS (PHOTOGRAMMETRIC DATA)
		UNDERGROUND STORM PIPE (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND ELECTRIC DUCT (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND GAS MAIN (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND SEWER FORCE MAIN (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND SEWER LINE (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND WATER MAIN (DOUBLE LINE 24 INCH AND OVER)
		BALANCED STONE WALL
		GUARD RAIL - STEEL POSTS
		GUARD RAIL - WOOD POSTS
		GUARD RAIL - DOUBLE FACE - STEEL POSTS
		GUARD RAIL - DOUBLE FACE - WOOD POSTS
		CHAIN LINK OR METAL FENCE
		WOOD FENCE
		HAY BALES/SILT FENCE
		TREE LINE
		SAWCUT LINE
		TOP OR BOTTOM OF SLOPE
		LIMIT OF EDGE OF PAVEMENT OR COLD PLANE AND OVERLAY
		BANK OF RIVER OR STREAM
		BORDER OF WETLAND
		100 FT WETLAND BUFFER
		200 FT RIVERFRONT BUFFER
		STATE HIGHWAY LAYOUT
		TOWN OR CITY LAYOUT
		COUNTY LAYOUT
		RAILROAD SIDELINE
		TOWN OR CITY BOUNDARY LINE
		PROPERTY LINE OR APPROXIMATE PROPERTY LINE
		EASEMENT

TRAFFIC SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
		CONTROLLER PHASE ACTUATED
		TRAFFIC SIGNAL HEAD (SIZE AS NOTED)
		WIRE LOOP DETECTOR (6' x 6' TYP UNLESS OTHERWISE SPECIFIED)
		VIDEO DETECTION CAMERA
		MICROWAVE DETECTOR
		PEDESTRIAN PUSH BUTTON, SIGN (DIRECTIONAL ARROW AS SHOWN) AND SADDLE
		EMERGENCY PREEMPTION CONFIRMATION STROBE LIGHT
		VEHICULAR SIGNAL HEAD
		VEHICULAR SIGNAL HEAD, OPTICALLY PROGRAMMED
		FLASHING BEACON
		PEDESTRIAN SIGNAL HEAD, (TYPE AS NOTED OR AS SPECIFIED)
		RAILROAD SIGNAL
		SIGNAL POST AND BASE (ALPHA-NUMERIC DESIGNATION NOTED)
		MAST ARM, SHAFT AND BASE (ARM LENGTH AS NOTED)
		HIGH MAST POLE OR TOWER
		SIGN AND POST
		SIGN AND POST (2 POSTS)
		MAST ARM WITH LUMINAIRE
		OPTICAL PRE-EMPTION DETECTOR
		CONTROL CABINET, GROUND MOUNTED
		CONTROL CABINET, POLE MOUNTED
		FLASHING BEACON CONTROL AND METER PEDESTAL
		LOAD CENTER ASSEMBLY
		PULL BOX 12"x12" (OR AS NOTED)
		ELECTRIC HANDHOLE 12"x24" (OR AS NOTED)
		TRAFFIC SIGNAL CONDUIT

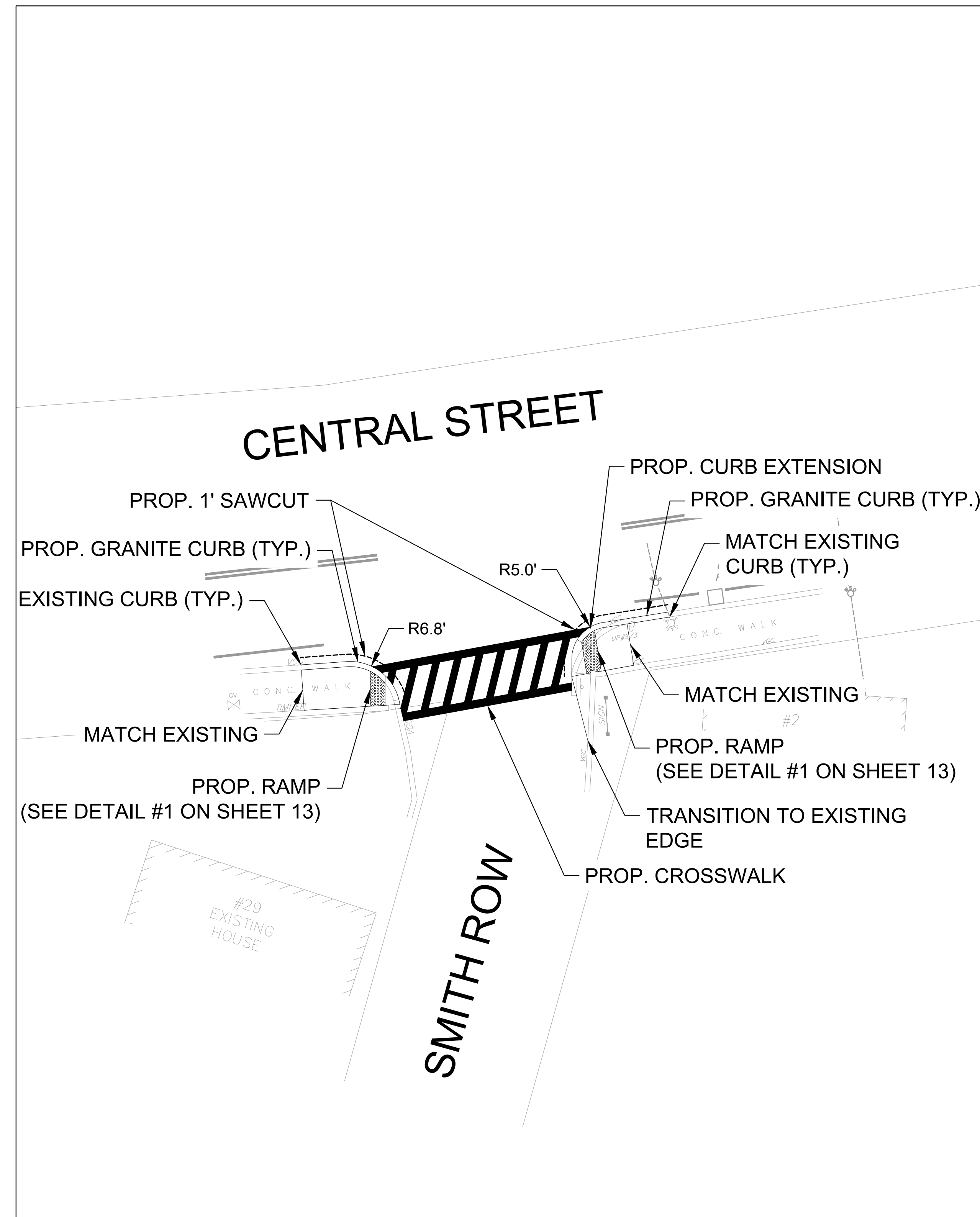
PAVEMENT MARKINGS SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
		PAVEMENT ARROW - WHITE
		LEGEND "ONLY" - WHITE
		STOP LINE
		CROSSWALK
		SOLID WHITE LINE - 6"
		SOLID YELLOW LINE - 6"
		BROKEN WHITE LINE - 6" (10' LINE SEGMENT AND 30' GAP)
		BROKEN YELLOW LINE - 6" (10' LINE SEGMENT AND 30' GAP)
		DOTTED WHITE LINE - 6" (3' LINE SEGMENT AND 9' GAP)
		DOTTED YELLOW LINE- 6" (3' LINE SEGMENT AND 9' GAP)
		DOTTED WHITE LINE EXTENSION - 6" (2' LINE SEGMENT AND 6' GAP)
		DOTTED YELLOW LINE EXTENSION - 6" (2' LINE SEGMENT AND 6' GAP)
		DOUBLE WHITE LINE - 6"
		DOUBLE YELLOW LINE - 6"

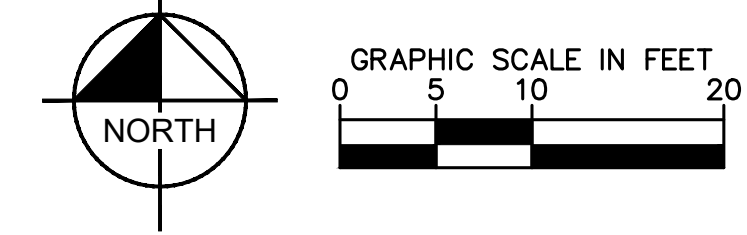










## CENTRAL STREET & SMITH ROW



SHEET NUMBER		4		COMPLETE STREETS PROJECTS PREPARED FOR TOWN OF HOLLISTON HOLLISTONMASSACHUSETTS		CONSTRUCTION PLANS		<div>KHA PROJECT 112613001</div> <div>DATE 09/29/2023</div> <div>SCALE AS SHOWN DESIGNED BYWJS DRAWN BYPAG CHECKED BYWJS</div> <div></div> <div></div> <table><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td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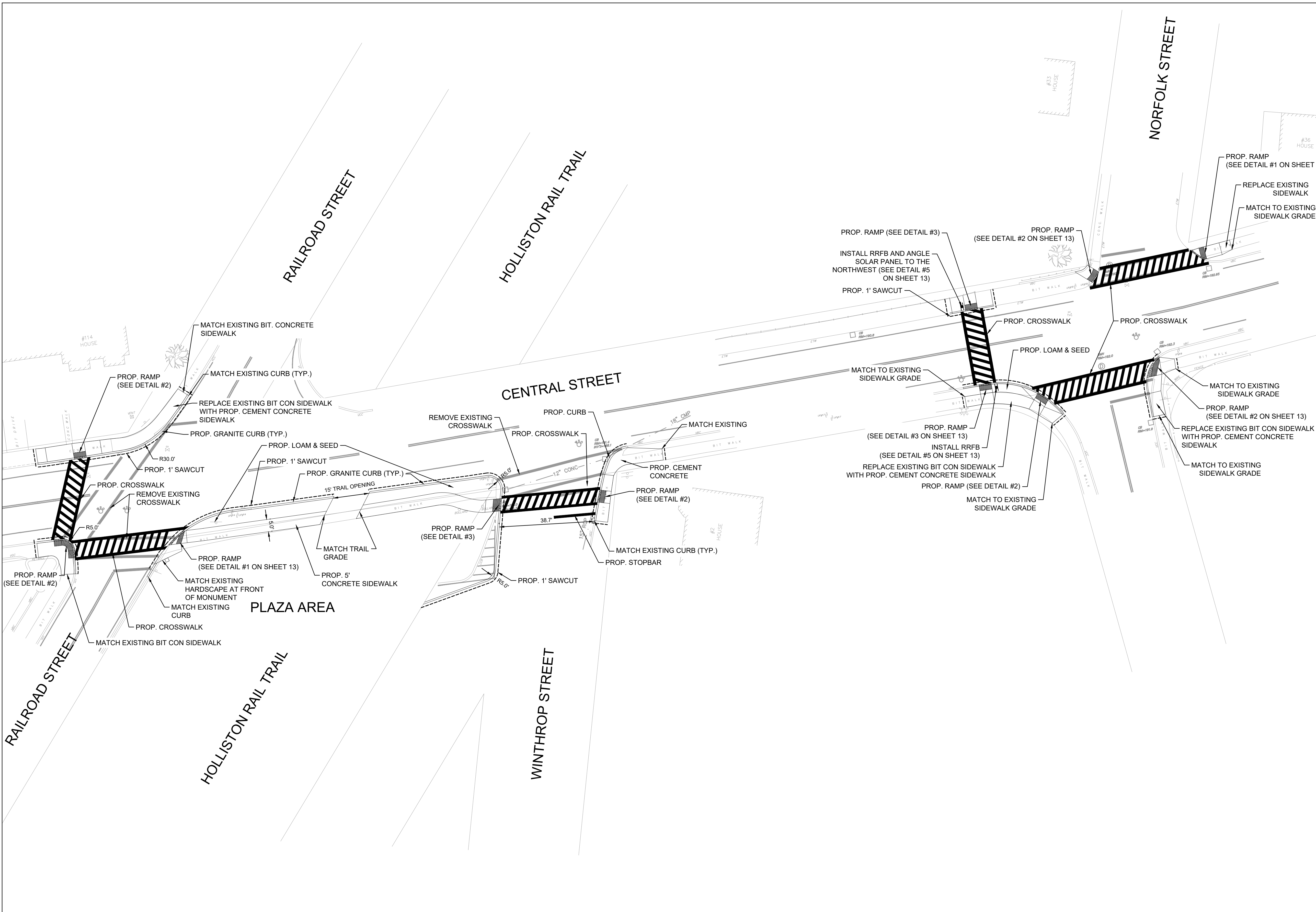




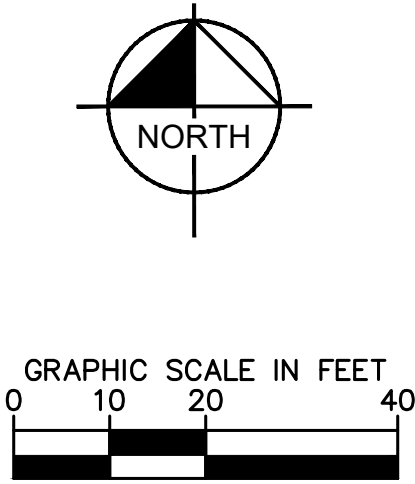





Plotted By: Graham, Payton Sheet Set: KIM Layout: 7 CONSTRUCTION PLANS September 28, 2023 03:32:40pm K:\BOS\TPT0112613001\_Holliston Tier 3 Eng-Const\CAD\Plots\Sheets\C-4.0 CONSTRUCTION PLANS.dwg  
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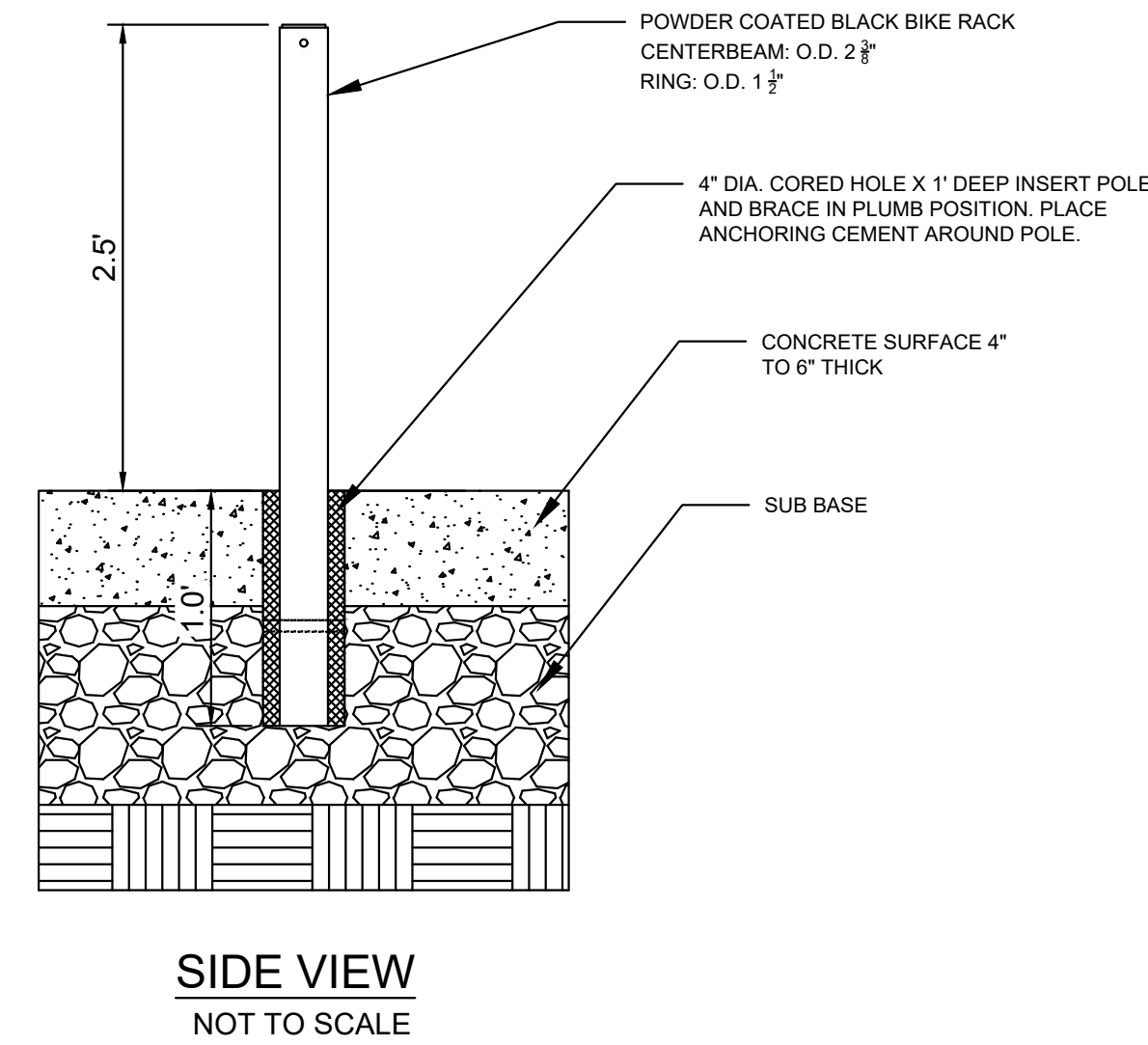


NOTE: SAWCUT OF EXISTING SIDEWALK REPAIR OR PATCH TO MATCH



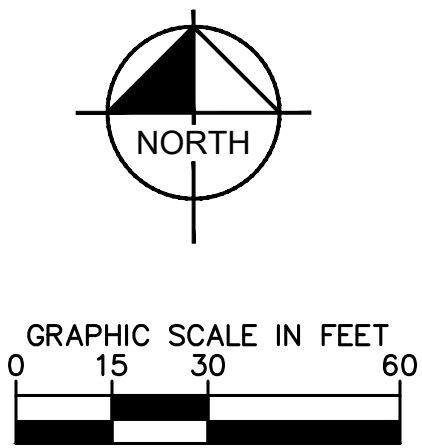
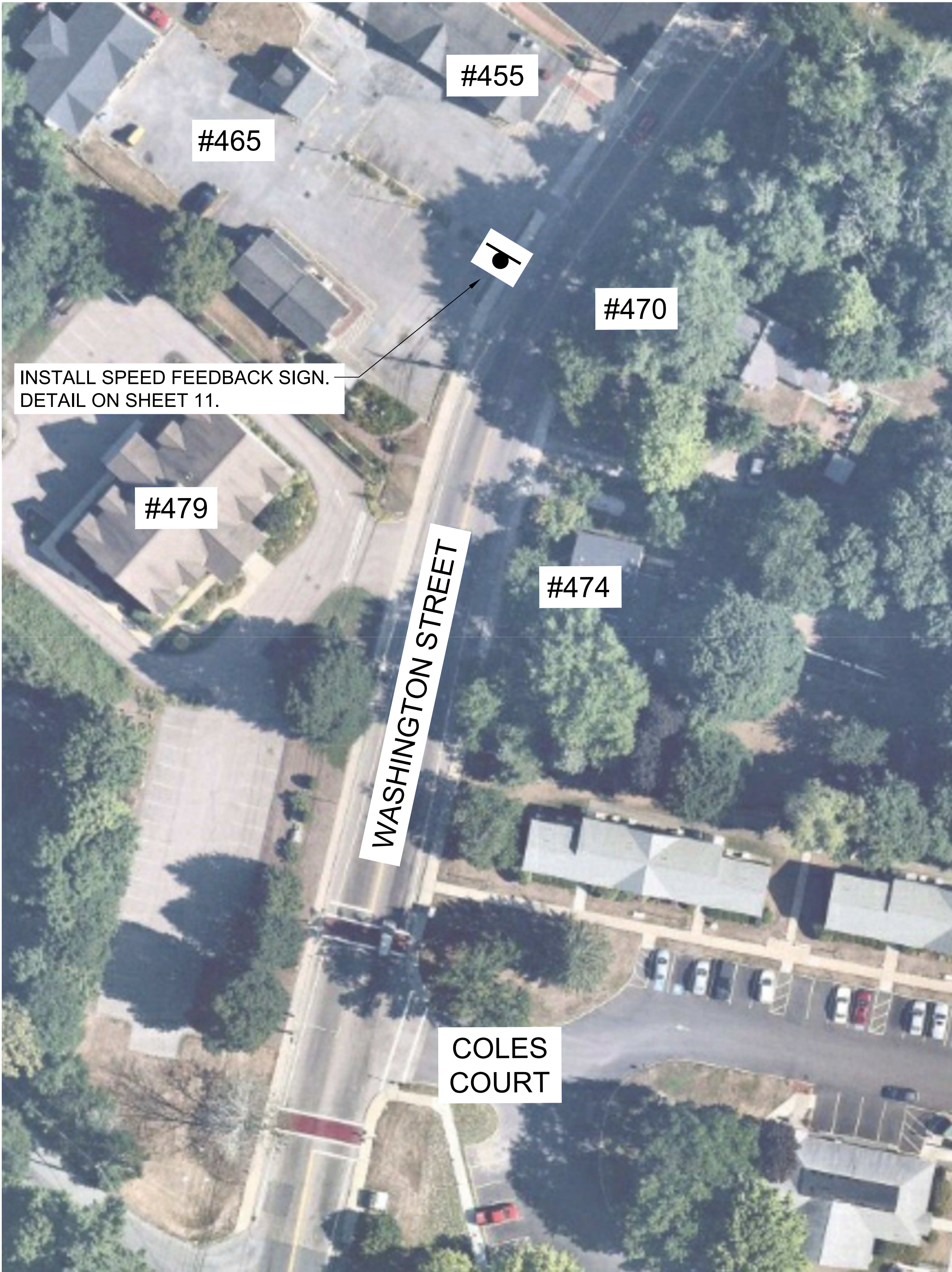
COMPLETE STREETS PROJECTS PREPARED FOR TOWN OF HOLLISTON HOLLISTON MASSACHUSETTS	CONSTRUCTION PLANS			KHA PROJECT 112613001 DATE 09/29/2023 SCALE AS SHOWN DESIGNED BY WJS DRAWN BY CHECKED BY	 © 2023 KIMLEY-HORN AND ASSOCIATES, INC. 404 WYMAN STREET, SUITE 385, WALTHAM, MA 02451 PHONE: 781-328-0676 WWW.KIMLEY-HORN.COM	REVISIONS	BY
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1. BICYCLE RACKS SHALL BE INSTALLED AT LEAST 3 FEET FROM ANY OBSTRUCTIONS AND AT LEAST 2 FEET FROM THE BACK OF CURB, AND ORIENTED PARALLEL TO THE ROADWAY.
2. CORE HOLES FOR CONCRETE FOOTINGS SHALL BE NO LESS THAN 4 INCHES IN DIAMETER AND NO LESS THAN 12 INCHES DEEP INTO THE GROUND.





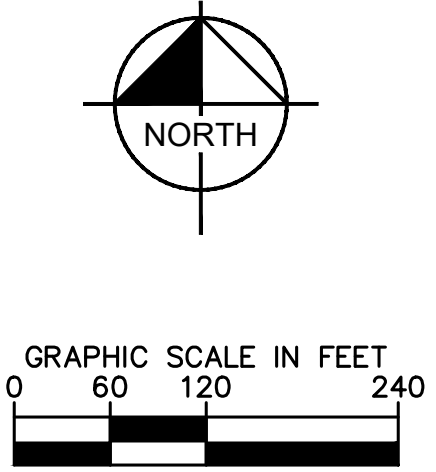
NOTES

1. ALL SIGN LOCATIONS SHALL BE VERIFIED BY THE TOWN OF HOLLISTON, THE ENGINEER, AND THE CONTRACTOR PRIOR TO INSTALLATION.

SHEET NUMBER <b>9</b>	COMPLETE STREETS PROJECTS PREPARED FOR TOWN OF HOLLISTON MASSACHUSETTS	SPEED FEEDBACK DISPLAY TWO LOCATIONS	KHA PROJECT 112613001 DATE 09/29/2023 SCALE AS SHOWN DESIGNED BY W/S DRAWN BY CHECKED BY W/S		 © 2023 KIMLEY-HORN AND ASSOCIATES, INC. 404 WYMAN STREET, SUITE 385, WALTHAM, MA 02451 PHONE: 781-328-0676 WWW.KIMLEY-HORN.COM	REVISIONS		BY
						No.	DATE	



Plotted By: Graham, Payton Sheet Set: K1a Layout: 10 SPEED FEEDBACK DISPLAYS HOLLIS STREET AND HOLLAND STREET September 28, 2023 03:35:01pm K:\BOS\_TPT\112513001\_Holliston\_Tier 3 Eng-Const\CAD\Plots\Sheets\C-MAPPING DIAGRAM.dwg  
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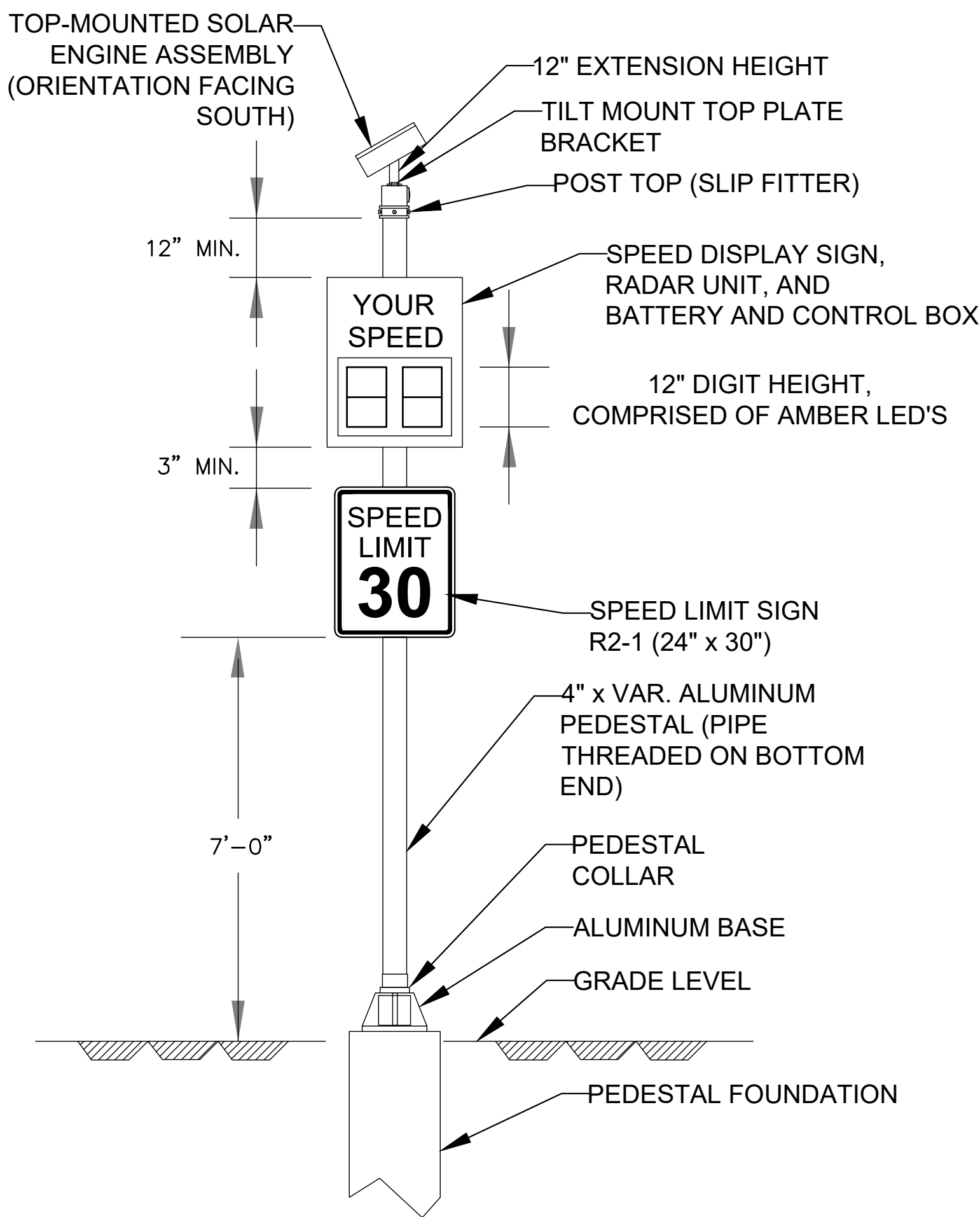
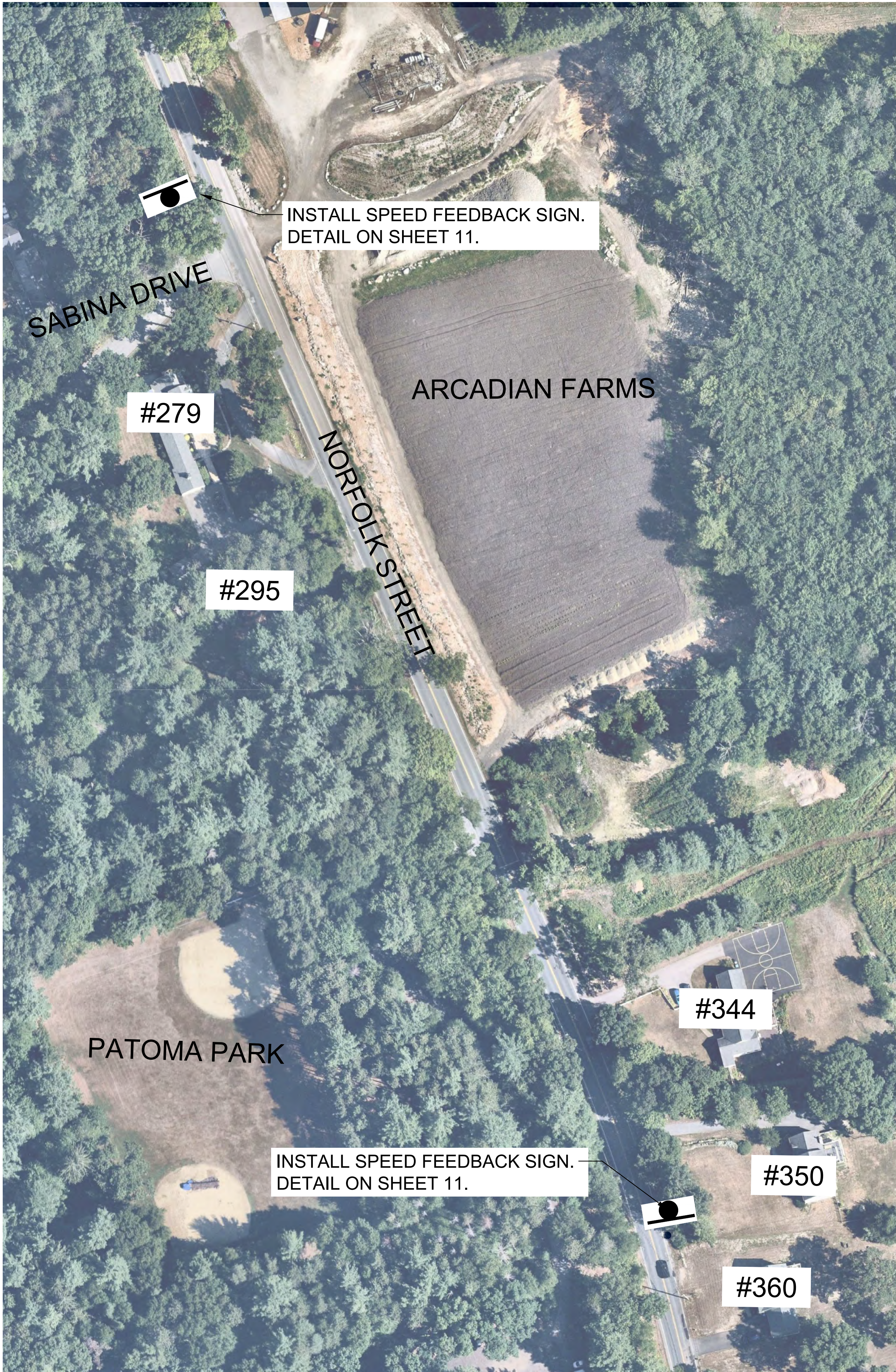
**NOTES**

1. ALL SIGN LOCATIONS SHALL BE VERIFIED BY THE TOWN OF HOLLISTON, THE ENGINEER, AND THE CONTRACTOR PRIOR TO INSTALLATION.

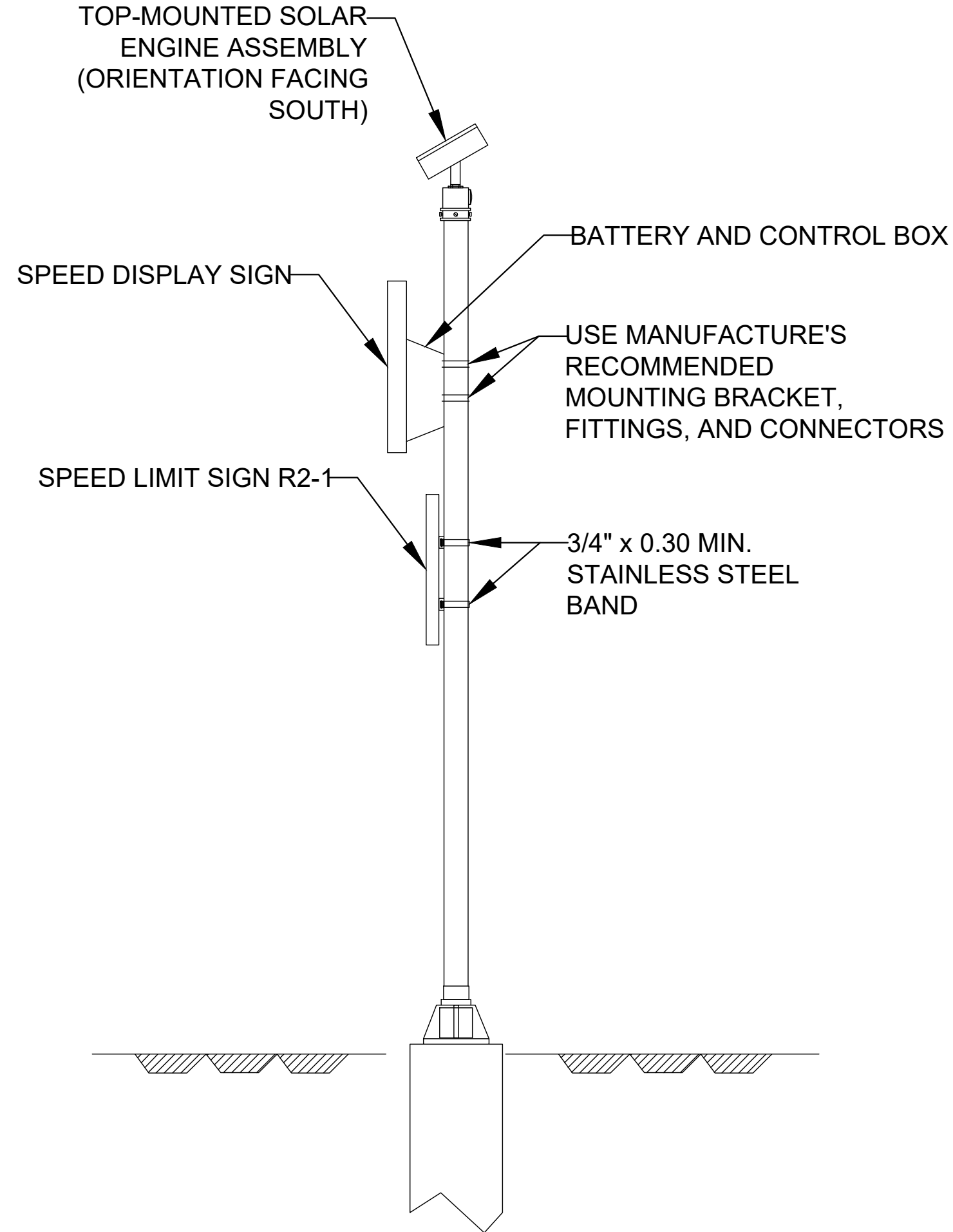
SHEET NUMBER		10	
COMPLETE STREETS PROJECTS PREPARED FOR TOWN OF HOLLISTON		MASSACHUSETTS	
SPEED FEEDBACK DISPLAYS HOLLIS STREET AND HIGHLAND STREET		KHA PROJECT 112513001 DATE 09/29/2023 SCALE AS SHOWN DESIGNED BY W/S DRAWN BY W/S CHECKED BY W/S	
TOWN OF HOLLISTON			
Kimley»Horn		© 2023 KIMLEY-HORN AND ASSOCIATES, INC. 404 WYMAN STREET, SUITE 385, WALTHAM, MA 02451 PHONE: 781-328-0676 WWW.KIMLEY-HORN.COM	
No.		REVISIONS	
		BY	
		DATE	



Plotted By: Graham, Payton Sheet Set: Kim Layout: MAPPING DIAGRAM (4) September 28, 2023 03:35:08pm K:\BOS TPT0112613001 Holliston Tier 3 Eng-Const\CAD\PlanSheets\C-MAPPING DIAGRAM.dwg  
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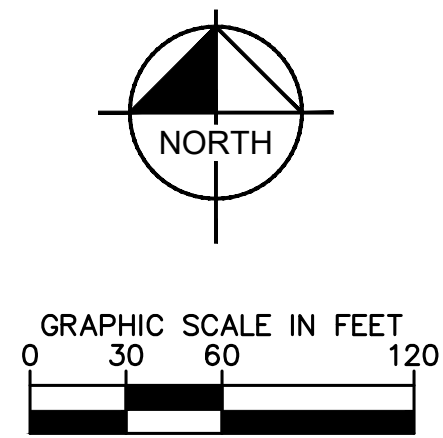
RADAR SPEED FEEDBACK SIGN - FRONT VIEW  
NOT TO SCALE





RADAR SPEED FEEDBACK SIGN - SIDE VIEW  
NOT TO SCALE

NOTES

1. ALL SIGN LOCATIONS SHALL BE VERIFIED BY THE TOWN OF HOLLISTON, THE ENGINEER, AND THE CONTRACTOR PRIOR TO INSTALLATION.


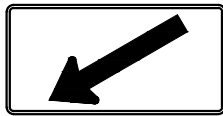



COMPLETE STREETS PROJECTS PREPARED FOR TOWN OF HOLLISTON MASSACHUSETTS	SPEED FEEDBACK DISPLAYS NORFOLK STREET	 TOWN OF HOLLISTON INCORPORATED DEC 3RD 1724	KHA PROJECT 112613001	DATE 09/29/2023	SCALE AS SHOWN	DESIGNED BY WJS	DRAWN BY WJS	CHECKED BY WJS	 Kimley-Horn © 2023 KIMLEY-HORN AND ASSOCIATES, INC. 404 WYMAN STREET, SUITE 385, WALTHAM, MA 02451 PHONE: 781-328-0676 WWW.KIMLEY-HORN.COM	No.	REVISIONS	DATE	BY	
SHEET NUMBER 11														









Plotted By:Graham, Payton Sheet Set:Kim Layout:SIGN SUMMARY September 28, 2023 03:35:45pm K:\BOS\_TPT0112613001\_Holliston\_Tier 3 Eng-Const\CAD\Plans\Sheets\C- SIGN SUMMARY.dwg  
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PERMANENT SIGNS

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR			POST SIZE AND NUMBER REQUIRED	UNIT AREA (S.F.)	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND	BORDER			
W11-2	30	30		SEE MUTCD STANDARD DETAIL			4	SEE MUTCD	STANDARD	DETAIL	MASSDOT SPEC. MOUNT ON POST	6.25	24
W16-7PR	24	12					4					2	8
W16-7PL	24	12					4					2	8

TEMPORARY SIGNS

IDENTIFI- CATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR			POST SIZE AND NUMBER REQUIRED	UNIT AREA (S.F.)	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK- GROUND	LEGEND	BORDER			
W5-1	30	30		SEE MUTCD STANDARD DETAIL			1	SEE MUTCD	STANDARD	DETAIL	MASSDOT SPEC. MOUNT ON POST	6.25	6.25
W13-1P	18	18					2					2.25	4.50
W20-4	30	30					2					6.25	12.50
W20-7	30	30					2					6.25	12.50
W20-7B	30	30					2					6.25	12.50
W21-5A	30	30					1					6.25	6.25

COMPLETE STREETS  
PROJECTS  
PREPARED FOR  
TOWN OF HOLLISTON

HOLLISTON  
MASSACHUSETTS

SIGN SUMMARY

KHA PROJECT  
112613001

DATE  
09/29/2023


SCALE AS SHOWN

DESIGNED BY W/S

DRAWN BY PAG

CHECKED BY W/S





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No.

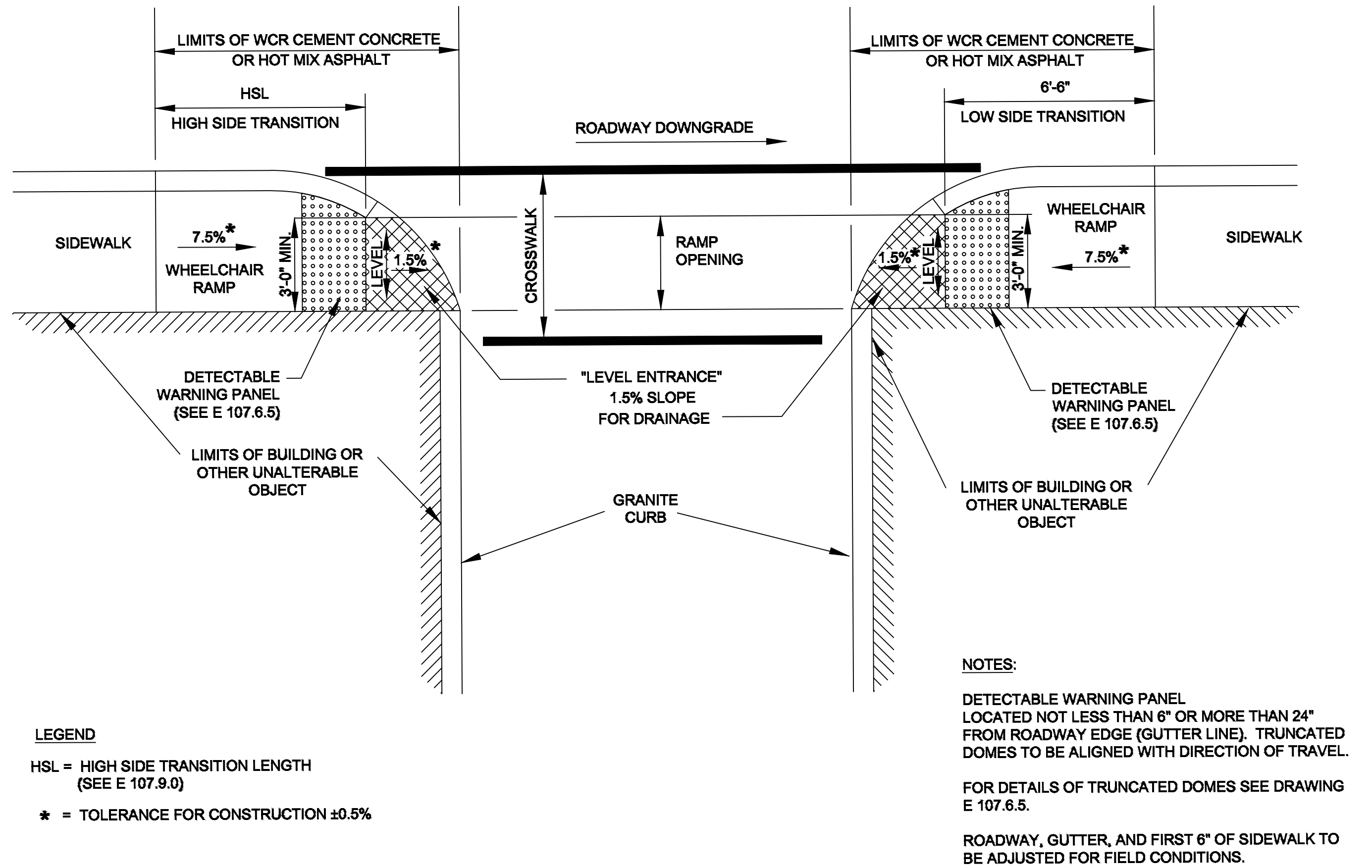
REVISIONS

DATE

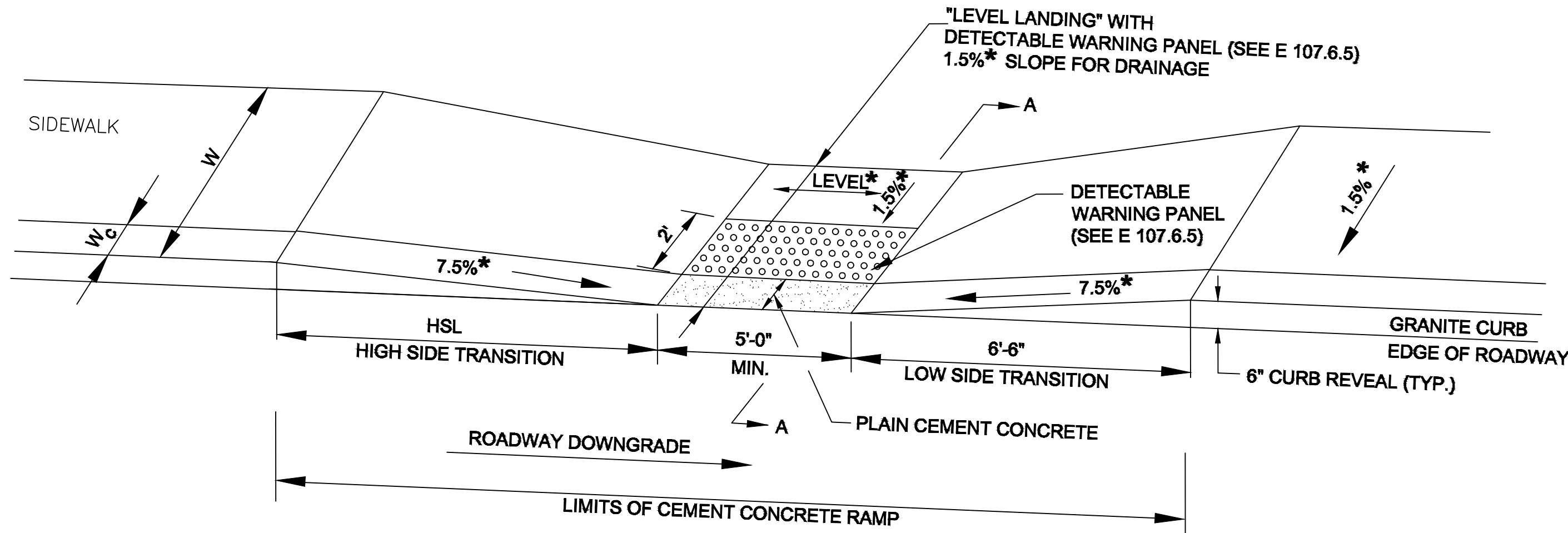
BY



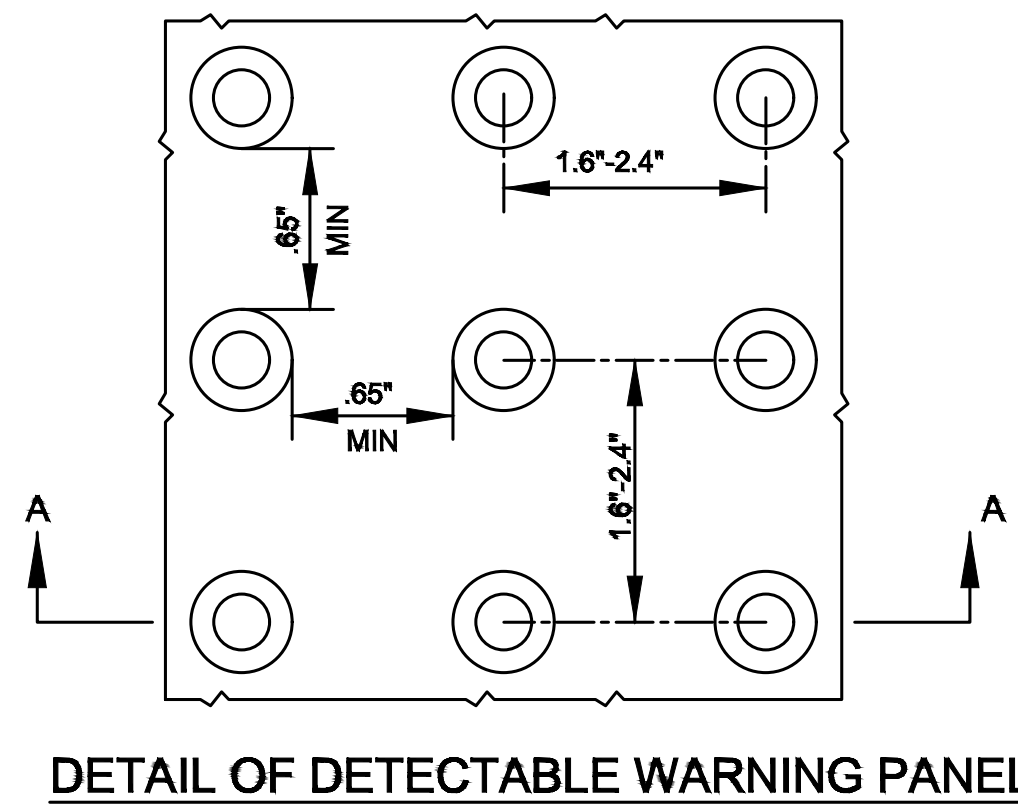
Plotted By: Graham, Payton Sheet Set: Kim Layout: CONSTRUCTION DETAILS September 28, 2023 09:35:52pm KUBOS TPT0112813001 Holliston Tier 3 Eng-ConstCADPlanSheetsIC-CONSTRUCTION DETAILS.dwg  
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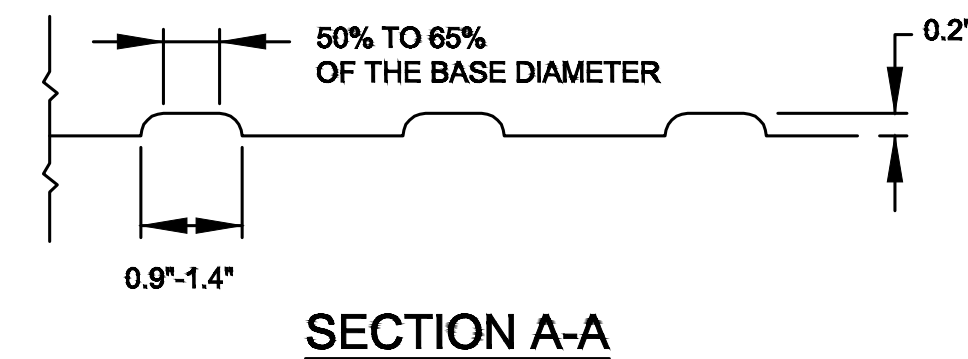
DETAIL #1  
WHEELCHAIR RAMP FOR ONE CONTINUOUS DIRECTION OF PEDESTRIAN TRAVEL  
NOT TO SCALE



DETAIL #2  
"T" INTERSECTION WHEELCHAIR RAMP  
NOT TO SCALE



DETAIL OF DETECTABLE WARNING PANEL



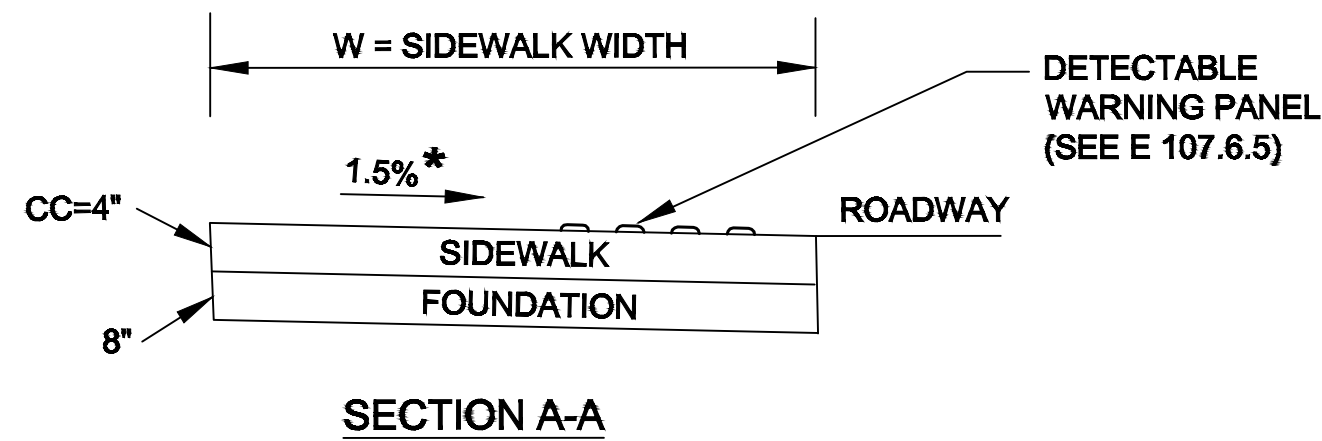
SECTION A-A

NOTE:  
PANELS MUST BE CAST IRON PERMANENTLY APPLIED TO THE RAMP. DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH ADJACENT WALKING SURFACES EITHER LIGHT-ON DARK, OR DARK-ON-LIGHT

DETAIL #4  
DETECTABLE WARNING PANEL FOR WHEELCHAIR RAMPS AND STANDARD RAMP TERMINOLOGY  
NOT TO SCALE

**LEGEND**  
HSL = HIGH SIDE TRANSITION LENGTH (SEE E 107.9.0)  
W = SIDEWALK WIDTH  
W<sub>c</sub> = CURB WIDTH  
CC = CEMENT CONCRETE  
\* = TOLERANCE FOR CONSTRUCTION ±0.5%  
USABLE SIDEWALK WIDTH PER AAB = W-W<sub>c</sub>  
USABLE SIDEWALK WIDTH PER AAB IS NOT TO BE LESS THAN 4'0"  
SEE E 107.6.5 FOR DETAILS OF DETECTABLE WARNING PANEL

NOTE:  
ROADWAY, GUTTER, AND FIRST 6" OF SIDEWALK TO BE ADJUSTED FOR FIELD CONDITIONS



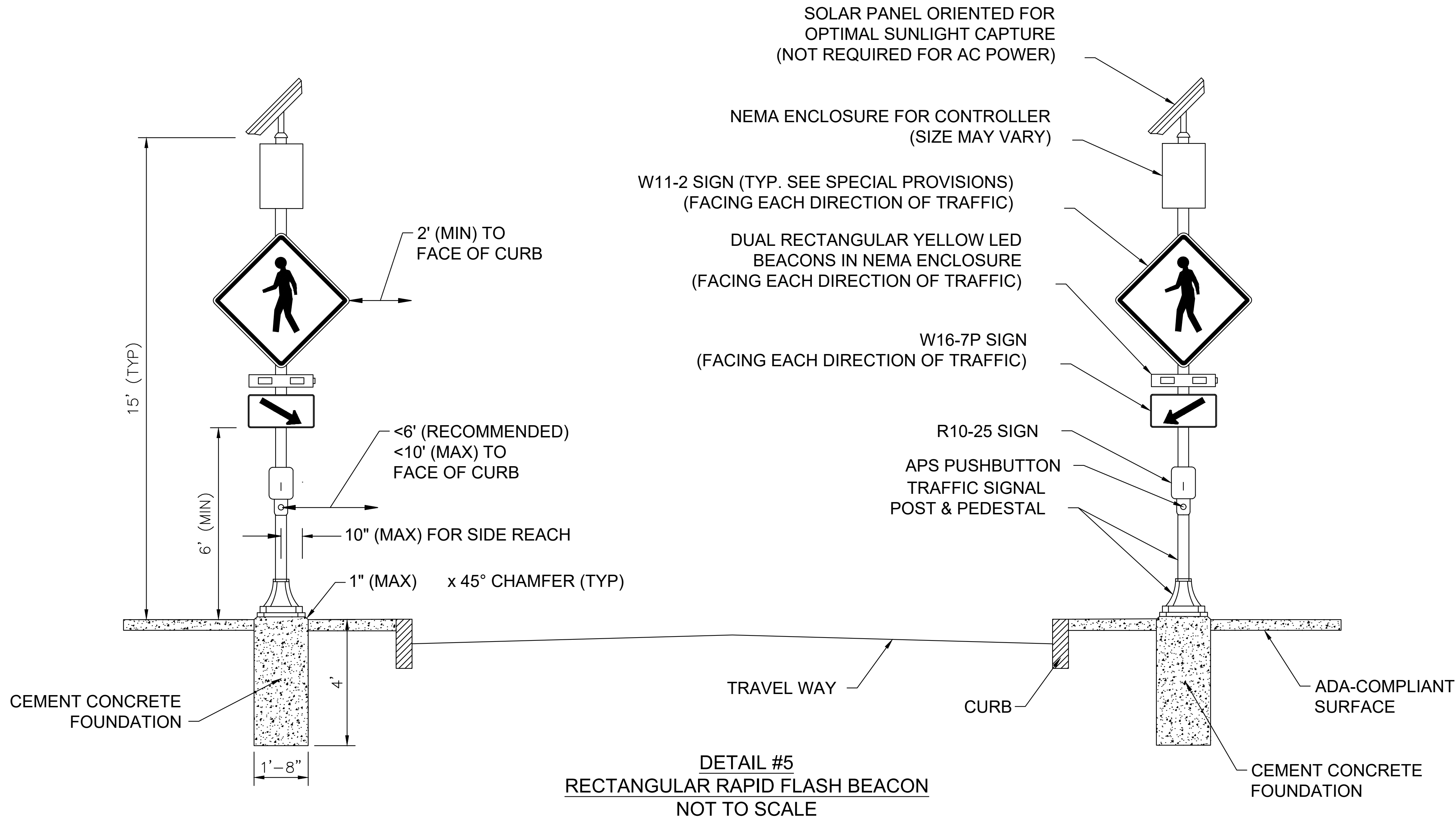
SECTION A-A

DETAIL #3  
WHEELCHAIR RAMP ON NARROW SIDEWALK WITH DETECTABLE WARNING PANEL  
NOT TO SCALE

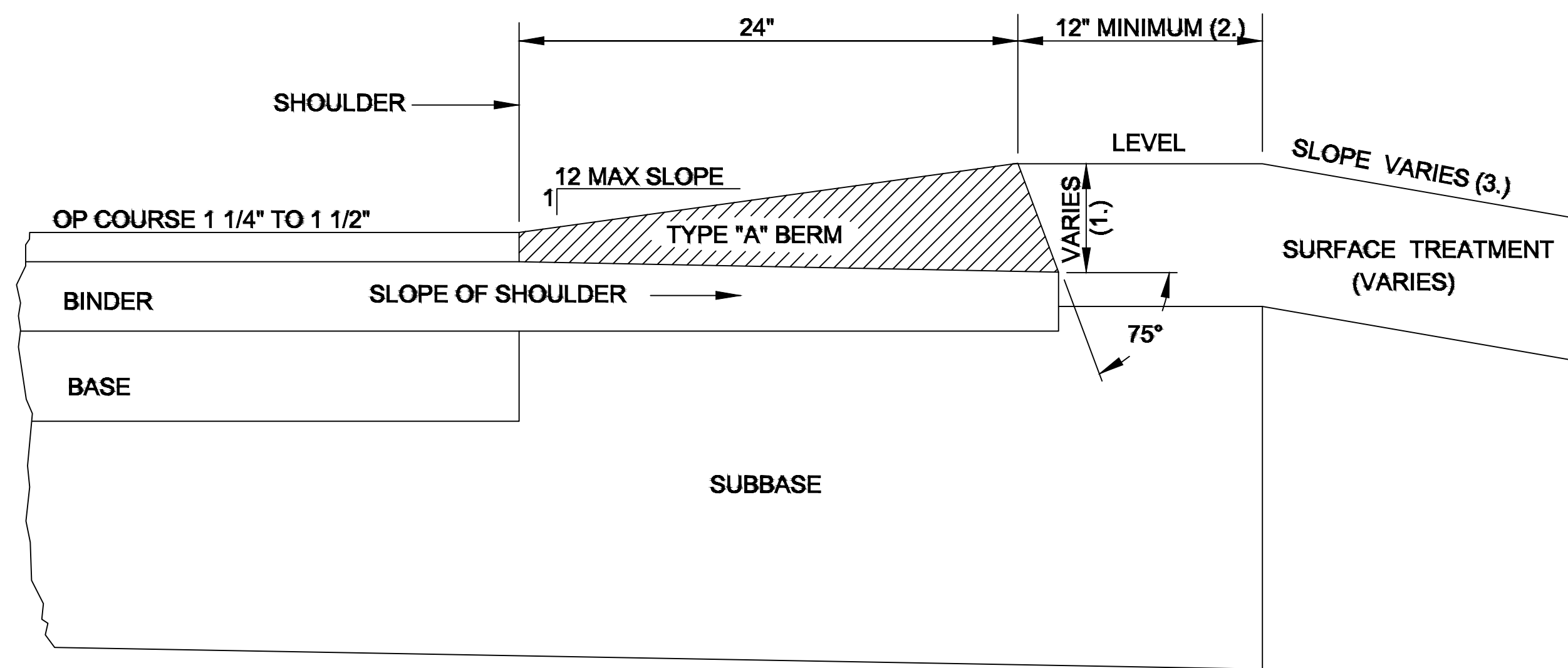
KHA PROJECT 112613001		DATE 09/29/2023	SCALE AS SHOWN	DESIGNED BY WJS	DRAWN BY WJS	CHECKED BY WJS	TOWN OF HOLLISTON INCORPORATED DECEMBER 3, 1851		Kimley»Horn © 2023 KIMLEY-HORN AND ASSOCIATES, INC. 404 WYMAN STREET, SUITE 385, WALTHAM, MA 02451 PHONE: 781-328-0676 WWW.KIMLEY-HORN.COM		REVISIONS		BY	DATE	
												No.			
CONSTRUCTION DETAILS												COMPLETE STREETS PROJECTS PREPARED FOR TOWN OF HOLLISTON MASSACHUSETTS			
SHEET NUMBER 13															



Plotted By: Graham, Payton, Street Set: Kim, Layout: CONSTRUCTION DETAILS (2), September 28, 2023, 03:35:57pm, KUBOS - TPT0112813001, Holliston Tier 3 Eng-Const-CAD/PlanSheets/C-CONSTRUCTION DETAILS.dwg, This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on the document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



DETAIL #5  
RECTANGULAR RAPID FLASH BEACON  
NOT TO SCALE

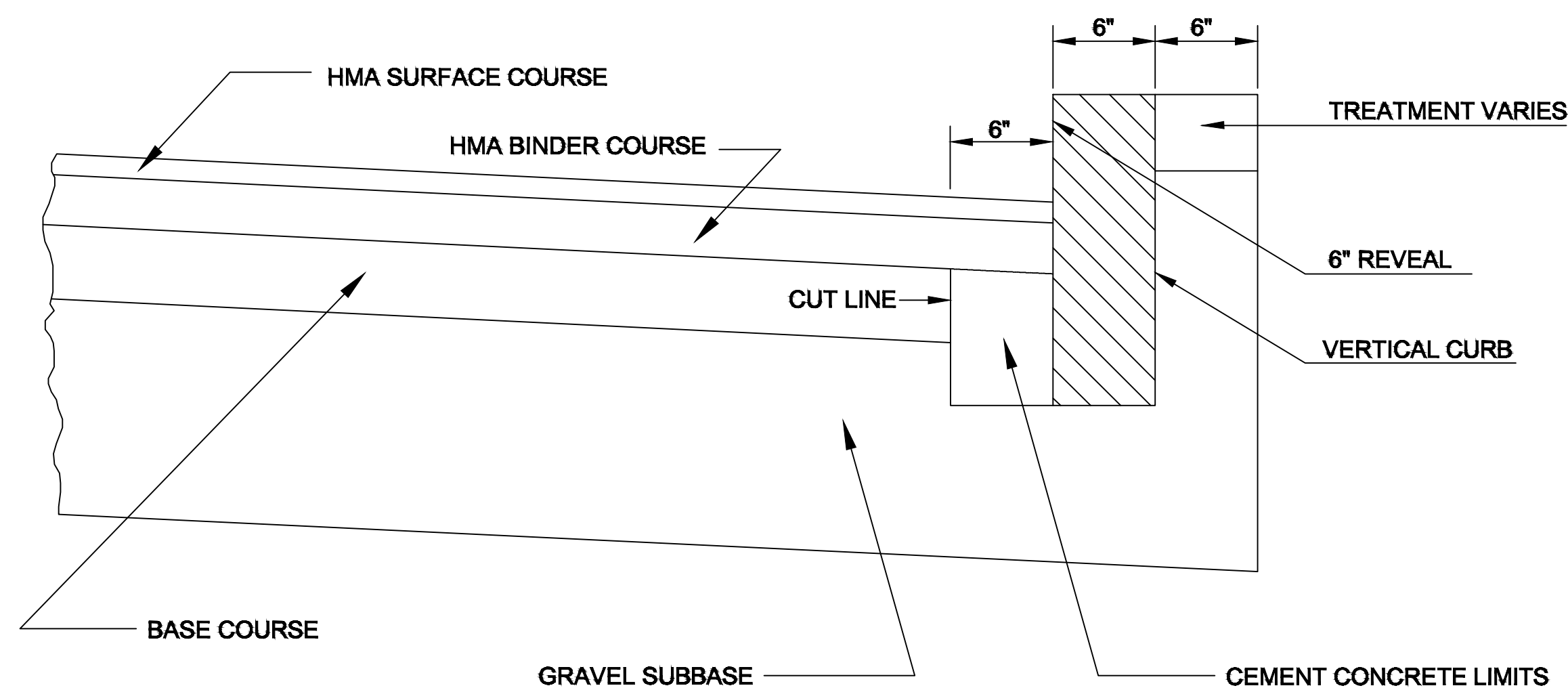


NOTE:

FOR MODIFIED BERM THE SLOPE REMAINS CONSTANT AT 1 (V) TO 12 (H)

1. THIS DIMENSION VARIES WITH THE THICKNESS OF THE TOP COURSE AND SLOPE OF BINDER
2. SEE E 401.1.1 FOR TYPICAL SECTION AT GUARDRAIL LOCATIONS
3. SEE TYPICAL SECTIONS FOR PROJECT



DETAIL #6  
CAPE COD BERM  
NOT TO SCALE



NOTES:

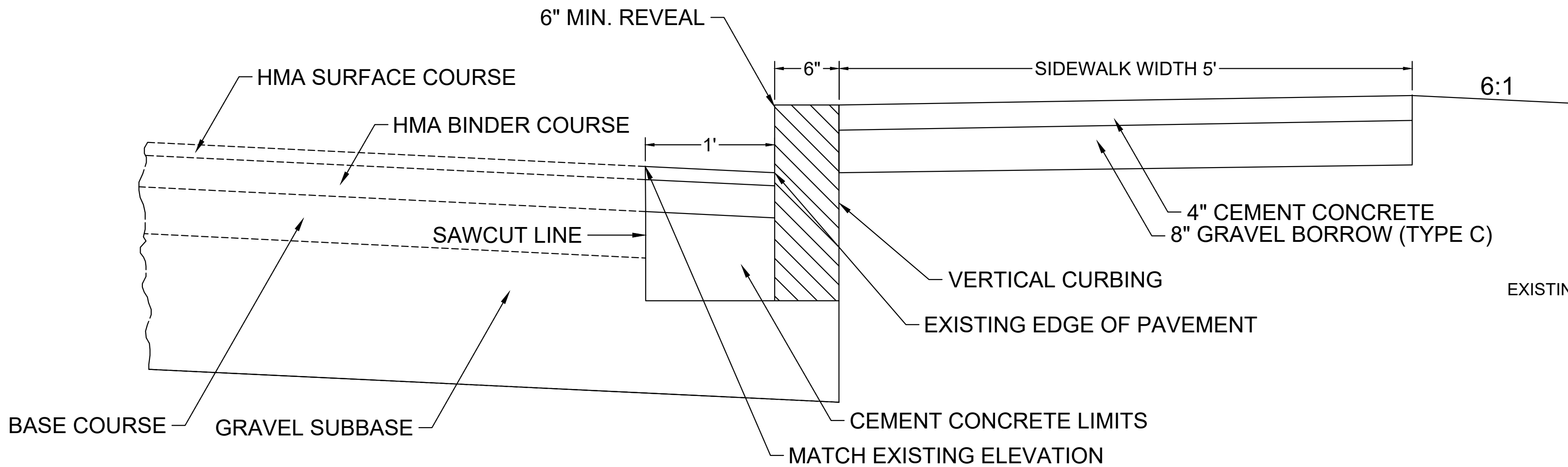
1. THIS PROCEDURE IS APPLICABLE ONLY IF CURB IS TO BE SET AFTER BASE COURSE IS IN PLACE PRIOR TO BINDER AND TOP PLACEMENT.
2. CUT NEAT LINE 6" FROM CURB LINE AND REMOVE BASE AND GRAVEL. REPLACE WITH CEMENT CONCRETE.
3. ANY DESIGNATED CEMENT CONCRETE THAT IS ACCEPTABLE UNDER SECTION M4 OF THE STANDARD SPECIFICATIONS MAY BE USED; ALL TEST REQUIREMENTS ARE WAIVED. HOT MIX ASPHALT SHALL NOT TO BE USED AS A SUBSTITUTE.

DETAIL #7  
VERTICAL GRANITE CURBING  
NOT TO SCALE

SHEET NUMBER		14		COMPLETE STREETS PROJECTS PREPARED FOR TOWN OF HOLLISTON		MASSACHUSETTS	
KHA PROJECT 112613001		DATE 09/29/2023		CONSTRUCTION DETAILS			
SCALE AS SHOWN		DESIGNED BY WJS					
DRAWN BY		PAG					
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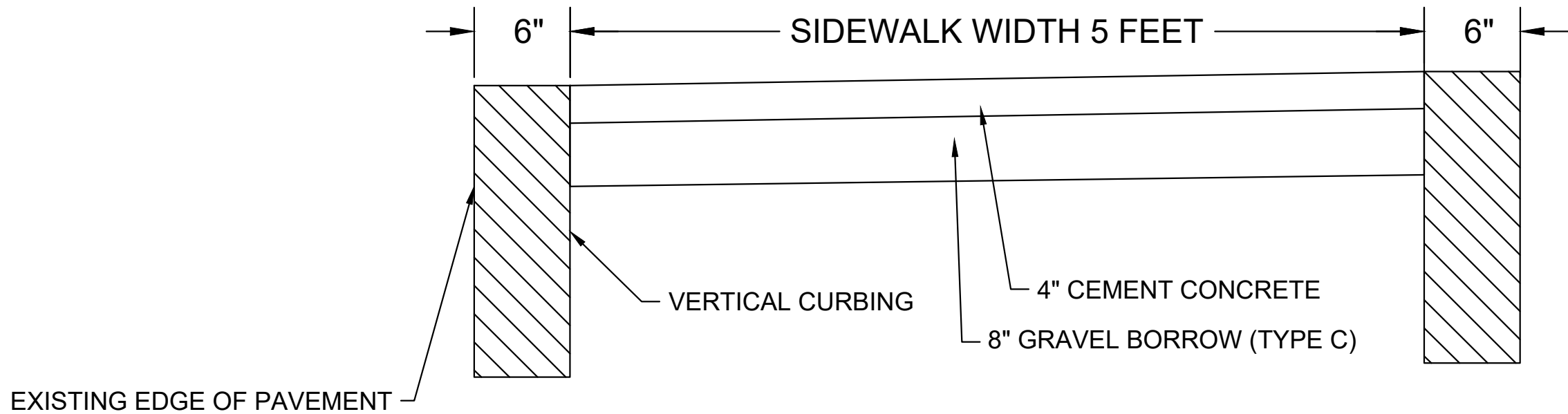
Plotted By: Graham, Peyton    Sheet Set: Kim    Layout: CONSTRUCTION DETAILS (3)    September 28, 2023    09:35:59pm    KUBOS    TPT0112813001    Holliston Tier 3 Eng-ConstructCADPlanSheetsC-CONSTRUCTION DETAILS.dwg    This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



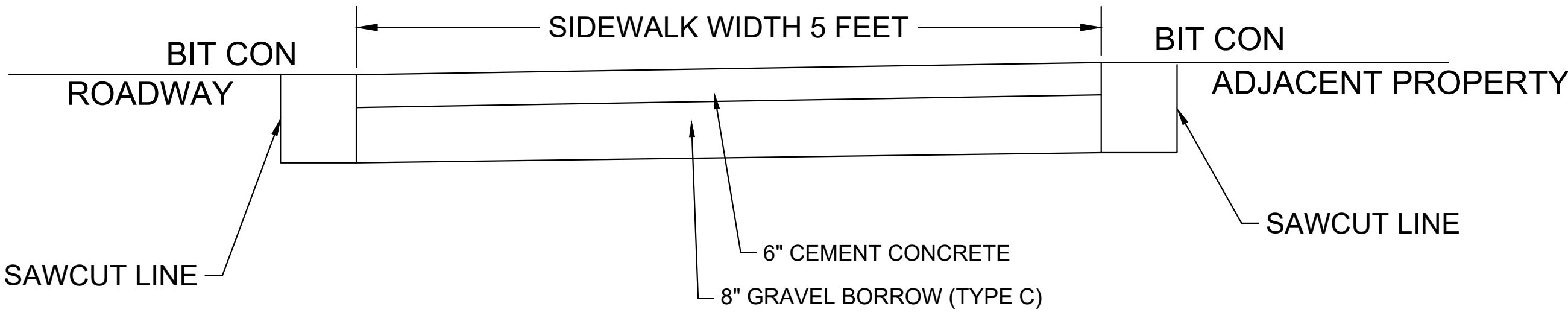
NOTES:

- CUT NEAT LINE 1' FROM CURB LINE AND REMOVE BASE AND GRAVEL. REPLACE WITH CEMENT CONCRETE.
- HOT MIX ASPHALT PATCH TO MATCH EXISTING ROADWAY AND CROSS SLOPES.
- ANY DESIGNATED CEMENT CONCRETE THAT IS ACCEPTABLE UNDER SECTION M4 OF THE STANDARD SPECIFICATION MAY BE USED; ALL TEST REQUIREMENTS ARE WAIVED. HOT MIX ASPHALT SHALL NOT BE USED AS A SUBSTITUTE

DETAIL #8  
TYPICAL SIDEWALK SECTION  
NOT TO SCALE



DETAIL #9  
SIDEWALK CROSS SECTION  
AREAS WHERE BACK OF SIDEWALK NEED CURB SUPPORT  
NOT TO SCALE



DETAIL #11  
SIDEWALK CROSS SECTION  
AREAS WHERE FLUSH SIDEWALK TO BE INSTALLED  
NOT TO SCALE

DETAIL #10  
TYPICAL SIDEWALK CROSS SECTION  
AREAS WHERE BACK OF SIDEWALK BLENDS TO  
EXISTING GRADE (LOAM OR HANDSCAPE)  
NOT TO SCALE



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KHA PROJECT	112613001
DATE	09/29/2023
SCALE	AS SHOWN
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CONSTRUCTION DETAILS

COMPLETE STREETS  
PROJECTS  
PREPARED FOR  
TOWN OF HOLLISTON  
MASSACHUSETTS



Plotted By:Graham, Payton, Sheet Set:Kim, Layout:16 TRAFFIC MANAGEMENT, September 28, 2023 03:36:01pm, K:\BOS\_T\TPO\112613001\_Holliston\_Tier 3 Edg-Const\CAD\PlanSheets\C-TRAFFIC MANAGEMENT.dwg, © 2023 KIMLEY-HORN AND ASSOCIATES, INC. All rights reserved. This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

NOTES:

1. ALL TEMPORARY TRAFFIC CONTROL WORK SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND ALL REVISIONS, UNLESS SUPERCEDED BY THESE PLANS.
2. ALL SIGN LEGENDS, BORDERS, AND MOUNTING SHALL BE IN ACCORDANCE WITH THE MUTCD.
3. TEMPORARY CONSTRUCTION SIGNING AND ALL OTHER TRAFFIC CONTROL DEVICES SHALL BE IN PLACE PRIOR TO THE START OF ANY WORK.
4. TEMPORARY CONSTRUCTION SIGNING, BARRICADES, AND ALL OTHER NECESSARY WORK ZONE TRAFFIC CONTROL DEVICES SHALL BE REMOVED FROM THE HIGHWAY OR COVERED WHEN THEY ARE NOT REQUIRED FOR CONTROL OF TRAFFIC.
5. SIGNS AND SIGN SUPPORTS LOCATED ON OR NEAR THE TRAVELED WAY, CHANNELIZING DEVICES, BARRIERS, AND CRASH ATTENUATORS MUST PASS THE CRITERIA SET FORTH IN NCHRP REPORT 350, "RECOMMENDED PROCEDURES FOR THE SAFETY PERFORMANCE EVALUATION OF HIGHWAY FEATURES" AND/OR "MANUAL FOR ASSESSING SAFETY HARDWARE" (MASH).
6. CONTRACTORS SHALL NOTIFY EACH ABUTTER AT LEAST 24 HOURS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE THE TEMPORARY CLOSURE OF ACCESS, SUCH AS CONDUIT INSTALLATION, EXISTING PAVEMENT EXCAVATION, TEMPORARY DRIVEWAY PAVEMENT PLACEMENT, AND SIMILAR OPERATIONS.
7. THE FIRST FIVE PLASTIC DRUMS OF A TAPER SHALL BE MOUNTED WITH TYPE A LIGHTS.
8. THE ADVISORY SPEED LIMIT, IF REQUIRED, SHALL BE DETERMINED BY THE ENGINEER.
9. DISTANCES ARE A GUIDE AND MAY BE ADJUSTED IN THE FIELD BY THE ENGINEER.
10. MAXIMUM SPACING OF TRAFFIC DEVICES IN A TAPER (DRUMS OR CONES) IS EQUAL IN FEET TO THE SPEED LIMIT IN MPH.
11. MINIMUM LANE WIDTH IS TO BE 11 FEET (3.3m) UNLESS OTHERWISE SHOWN. MINIMUM LANE WIDTH TO BE MEASURED FROM THE EDGE OF DRUMS OR MEDIAN BARRIER.
12. ALL SIGNS SHALL BE MOUNTED ON THEIR OWN STANDARD SIGN SUPPORTS.

LEGEND:

●

REFLECTORIZED PLASTIC DRUM OR 36" CONE

▨

WORK ZONE

WORK VEHICLE

P/F

POLICE/FLAGGER DETAIL

➔

DIRECTION OF TRAFFIC

TRUCK MOUNTED ATTENUATOR

▨

TYPE III BARRICADE

IMPACT ATTENUATOR

TRAFFIC OR PEDESTRIAN SIGNAL

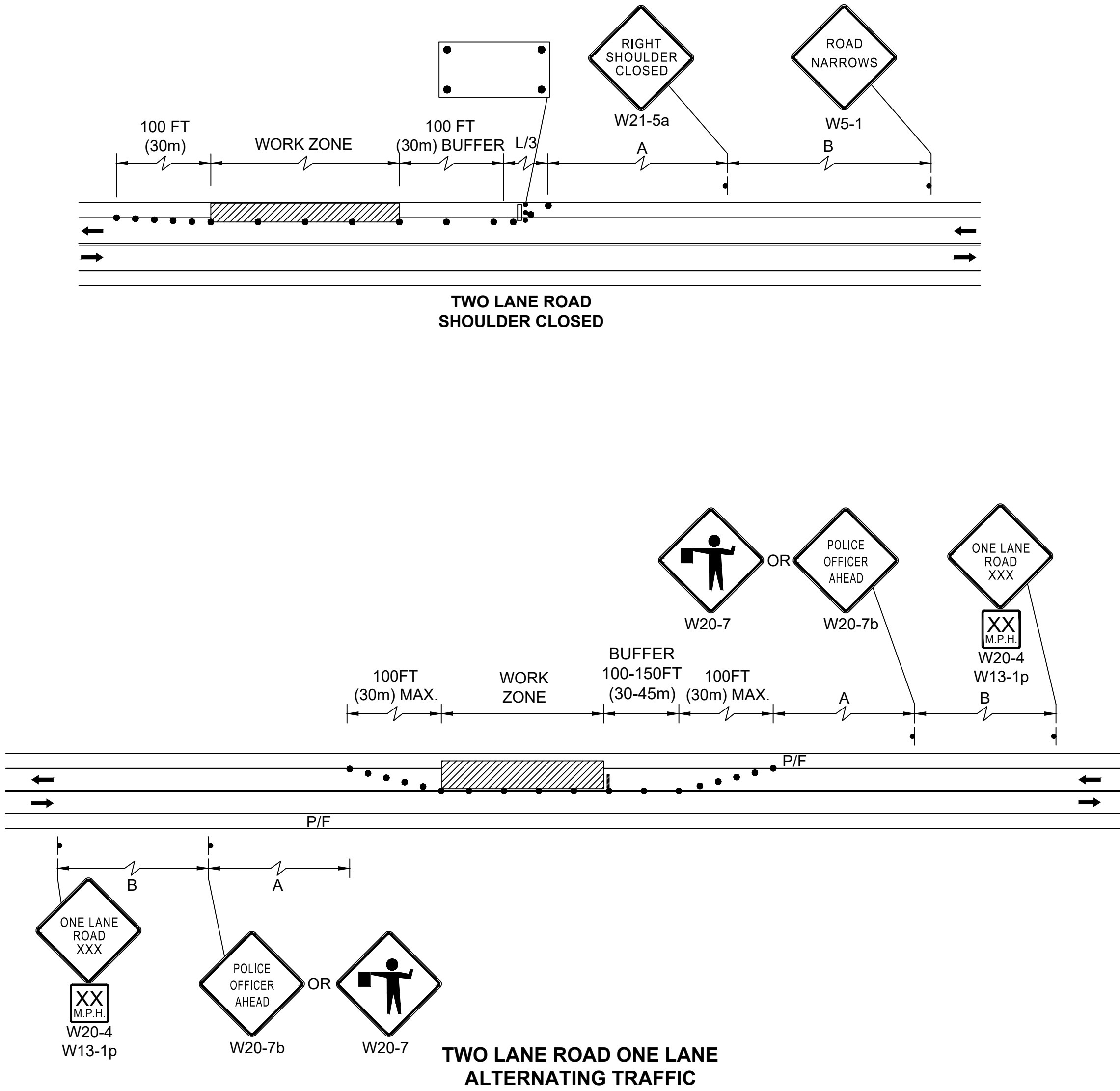
CHANGEABLE MESSAGE SIGN

MEDIAN BARRIER

SIGN

ARROW BOARD

MEDIAN BARRIER WITH WARNING LIGHTS



COMPLETE STREETS PROJECTS  
PREPARED FOR  
TOWN OF HOLLISTON  
HOLLISTON  
MASSACHUSETTS

TRAFFIC MANAGEMENT

KHA PROJECT  
112613001

DATE  
09/29/2023

SCALE  
AS SHOWN

DESIGNED BY  
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