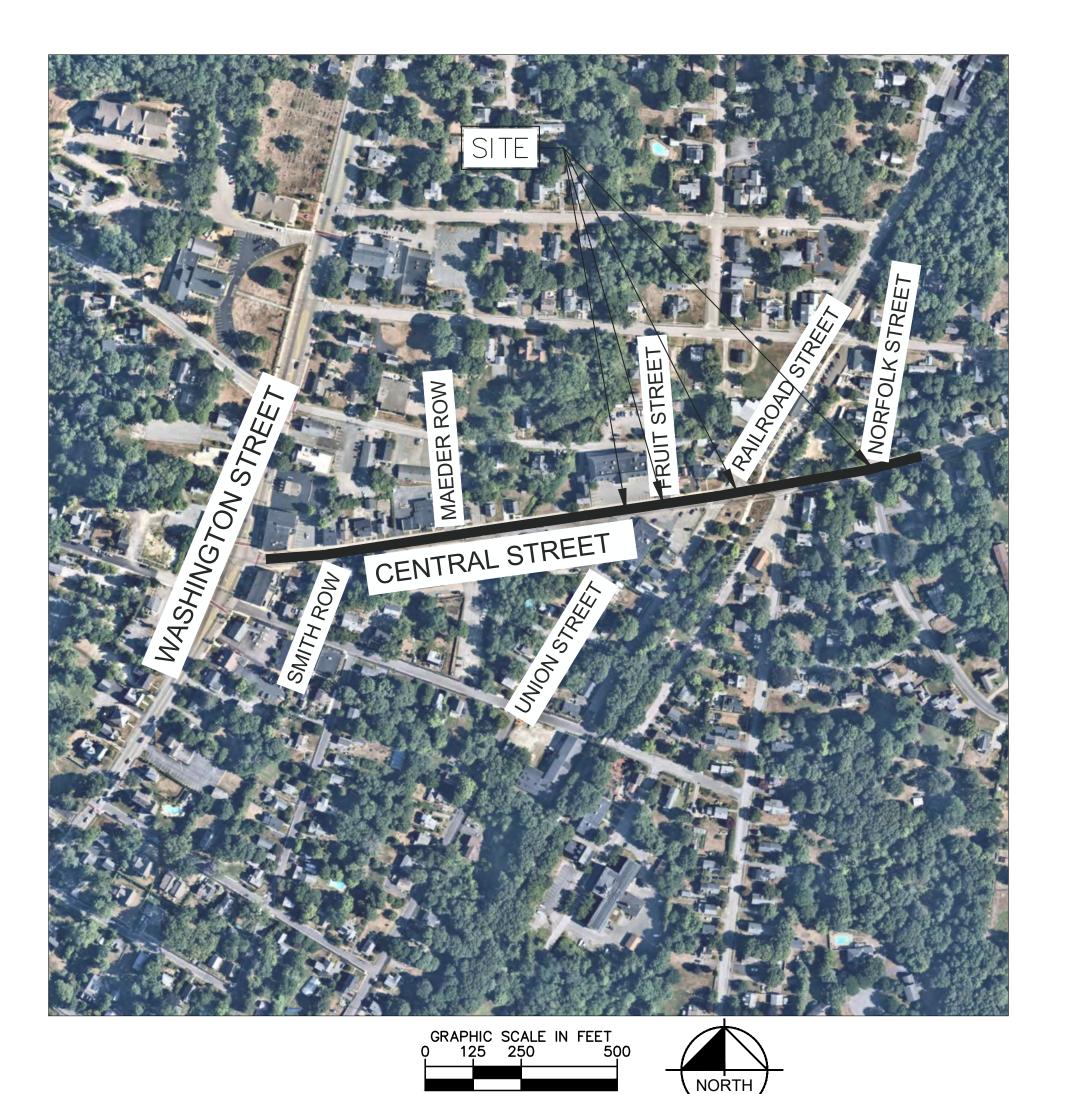
COMPLETE STREETS PROJECT CENTRAL STREET AND OTHER LOCATIONS

HOLLISTON, MASSACHUSETTS

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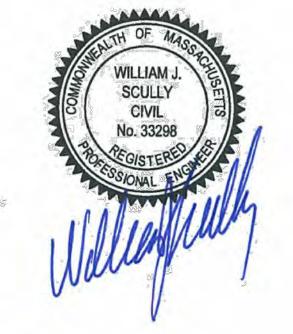
THE MASSACHUSETTS HIGHWAY DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES DATED 2023, THE 2017 CONSTRUCTION STANDARD DETAILS, THE 2009 MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS WITH MASSACHUSETTS AMENDMENTS, THE 1990 STANDARD DRAWINGS FOR SIGNS AND SUPPORTS, THE 1968 STANDARD DRAWINGS FOR TRAFFIC SIGNALS AND HIGHWAY LIGHTING AND THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, AND WHERE APPLICABLE, THE TOWN OF HOLLISTON STANDARDS WILL GOVERN.

PROJECT PROPONENT

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PROJECT TEAM

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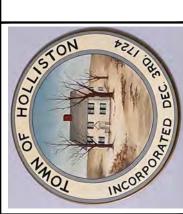
SCALE AS SHOWN
DESIGNED BY WJS

OVER SHEET

PROJECTS
PREPARED FOR
IN OF HOLLISTON

ABBREVIATIONS GENERAL		ABBREVIATIONS (cont.)		GENERAL S	YMBOLS		TRAFFIC SYMBOLS						
<u>GENERAL</u> AADT	ANNUAL AVERAGE DAILY TRAFFIC	GENERAL	<u>L</u>	EXISTING	PROPOSED	DESCRIPTION	EXISTING	PROPOSED	DESCRIPTION				
BAN	ABANDON	PI	POINT OF INTERSECTION	□ JB □ ⊕ ⊕ CB	JB CB	JERSEY BARRIER CATCH BASIN	<i>Ø</i> 1	<i>Ø</i> 1	CONTROLLER PHASE ACTUATED				
OJ PPROX.	ADJUST APPROXIMATE	POC	POINT ON CURVE		CB	CATCH BASIN CURB INLET	[6]						
;.	ASPHALT CONCRETE	POT PRC	POINT ON TANGENT POINT OF REVERSE CURVATURE	⊕ FP	⊕ FP	FLAG POLE			TRAFFIC SIGNAL HEAD (SIZE AS NOTED)				
CM PIPE	ASPHALT COATED CORRUGATED METAL PIPE	PROJ	PROJECT	G GP	G GP	GAS PUMP			WIRE LOOP DETECTOR (6' x 6' TYP UNLESS OTHERWISE SPECIFIED)				
RM ·	CAP COD BERM	PROP	PROPOSED	□ MB	□ MB	MAIL BOX POST SQUARE	L _ J		· ·				
	BITUMINOUS BOTTOM OF CURB	PSB PT	PLANTABLE SOIL BORROW	0	\bigcirc	POST CIRCULAR	72	T	VIDEO DETECTION CAMERA				
	BOUND	PVC	POINT OF TANGENCY POINT OF VERTICAL CURVATURE	⊕ WELL	⊕ WELL	WELL		>=	MICROWAVE DETECTOR				
_	BASELINE	PVI	POINT OF VERTICAL INTERSECTION	□ EHH	□ EHH	ELECTRIC HANDHOLE	\oplus	•	PEDESTRIAN PUSH BUTTON, SIGN (DIRECTIONAL ARROW AS SHOW AND SADDLE				
)G	BUILDING BENCHMARK	PVT	POINT OF VERTICAL TANGENCY	→ GG	⊖ GG	FENCE GATE POST GAS GATE	*	*	EMERGENCY PREEMPTION CONFIRMATION STROBE LIGHT				
	BY OTHERS	PVMT PWW	PAVEMENT PAVED WATER WAY	BHL #	⊕ BHL #	BORING HOLE	<	—	VEHICULAR SIGNAL HEAD				
S	BOTTOM OF SLOPE	R	RADIUS OF CURVATURE	→ MW #	→ MW #	MONITORING WELL	< </td <td>-</td> <td>VEHICULAR SIGNAL HEAD, OPTICALLY PROGRAMMED</td>	-	VEHICULAR SIGNAL HEAD, OPTICALLY PROGRAMMED				
	BRIDGE	R&D	REMOVE AND DISPOSE	■ TP #	■ TP#	TEST PIT	←	—	FLASHING BEACON				
CI	CATCH BASIN CATCH BASIN WITH CURB INLET	RCP	REINFORCED CONCRETE PIPE	Ψ Ψ	У	HYDRANT LIGHT POLE							
<i>)</i>	CEMENT CONCRETE	RD RDWY	ROAD ROADWAY	平 CO.BD.	*	COUNTY BOUND	□ RRSG	_	PEDESTRIAN SIGNAL HEAD, (TYPE AS NOTED OR AS SPECIFIED)				
М	CEMENT CONCRETE MASONRY	REM	REMOVE			GPS POINT	1	⊠ RRSG	RAILROAD SIGNAL				
M	CEMENT	RET	RETAIN	©	©	CABLE MANHOLE	-∳- OR O	•	SIGNAL POST AND BASE (ALPHA-NUMERIC DESIGNATION NOTED)				
)	CURB INLET CAST IRON PIPE		RETAINING WALL RIGHT OF WAY	(D)	(b)	DRAINAGE MANHOLE ELECTRIC MANHOLE	00	€ 20'	MAST ARM, SHAFT AND BASE (ARM LENGTH AS NOTED)				
=	CHAIN LINK FENCE	ROW RR	RAILROAD	(G)	(G)	GAS MANHOLE			HIGH MAST POLE OR TOWER				
	CENTERLINE	R&R	REMOVE AND RESET	M	M	MISC MANHOLE			SIGN AND POST				
P	CORRUGATED METAL PIPE	R&S	REMOVE AND STACK	<u>(S)</u>	<u>s</u>	SEWER MANHOLE	00	$\overline{\bigcirc}$	SIGN AND POST (2 POSTS)				
P	CORRUGATED STEEL PIPE COUNTY	RT SB	RIGHT STONEBOUND	(T)	(T) (W)	TELEPHONE MANHOLE WATER MANHOLE		₩ 20′	MAST ARM WITH LUMINAIRE				
NC	CONCRETE	SHLD	SHOULDER	■ MHB	■ MHB	MASSACHUSETTS HIGHWAY BOUND		─					
NT	CONTINUOUS	SMH	SEWER MANHOLE	- MON		MONUMENT			OPTICAL PRE-EMPTION DETECTOR				
NST CD	CONSTRUCTION	ST STA	STREET STATION	□ SB		STONE BOUND			CONTROL CABINET, GROUND MOUNTED				
GR '	CROWN GRADE DESIGN HOURLY VOLUME	SSD	STATION STOPPING SIGHT DISTANCE	■ TB		TOWN OR CITY BOUND TRAVERSE OR TRIANGULATION STATION			CONTROL CABINET, POLE MOUNTED				
	DROP INLET	SHLO	STATE HIGHWAY LAYOUT LINE	⊸ TPL or GUY	→ TPL or GUY	TROLLEY POLE OR GUY POLE			FLASHING BEACON CONTROL AND METER PEDESTAL				
	DIAMETER	SW	SIDEWALK	o HTP		TRANSMISSION POLE		\bowtie	LOAD CENTER ASSEMBLY				
	DUCTILE IRON PIPE STEADY DON'T WALK - PORTLAND ORANGE	Т	TANGENT DISTANCE OF CURVE/TRUCK %	-&- UFB	-b- UFB	UTILITY POLE W/ FIREBOX			PULL BOX 12"x12" (OR AS NOTED)				
Y	DRIVEWAY	TAN	TANGENT	-\$- UPDL	-∳ UPDL	UTILITY POLE WITH DOUBLE LIGHT UTILITY POLE W / 1 LIGHT			ELECTRIC HANDHOLE 12"x24" (OR AS NOTED)				
1	ELECTRIC HANDHOLE	TEMP	TEMPORARY	>- ♦ ULT -0- UP	»—∳ ULT ⊸– UP	UTILITY POLE W/ 1 LIGHT		=====	TRAFFIC SIGNAL CONDUIT				
EV (or EL.)	ELEVATION	TC TMH	TOP OF CURB TELEPHONE MANHOLE			BUSH		=====	TRAFFIC SIGNAL CONDUIT				
1B	EMBANKMENT ELECTRIC MANHOLE	TOS	TOP OF SLOPE	•SIZE & TYPE		TREE							
H	1 1 1 (211)(210)(210)(311)(211)		TYPICAL	0		STUMP SWAMP / MARSH	PAVEMEN	T MARKING	GS SYMBOLS				
		TYP		V 2		SWAMP / MARSH			OC CT MIDGES				
D	EDGE OF PAVEMENT ELECTRIC TRANSFORMER	UP	UTILITY POLE	₩G	- ∳ - WG		-						
	EDGE OF PAVEMENT ELECTRIC TRANSFORMER EXISTING		UTILITY POLE VARIES VERTICAL	→ WG • PM	→ WG ∘ PM	WATER GATE PARKING METER	EXISTING	PROPOSED	DESCRIPTION				
P ST (or EX	EDGE OF PAVEMENT ELECTRIC TRANSFORMER EXISTING EXCAVATION	UP VAR	VARIES	• РМ —— он ———	. РМ —— он ——	WATER GATE PARKING METER OVERHEAD CABLE/WIRE	-		DESCRIPTION PAVEMENT ARROW - WHITE				
Р	EDGE OF PAVEMENT ELECTRIC TRANSFORMER EXISTING	UP VAR VERT VC VGC	VARIES VERTICAL VERTICAL CURVE VERTICAL GRANITE CURBING	• РМ —— он —————————————————————————————————	• РМ —— он ——	WATER GATE PARKING METER OVERHEAD CABLE/WIRE CURBING	-						
ST (or EX	EDGE OF PAVEMENT ELECTRIC TRANSFORMER EXISTING EXCAVATION FRAME AND COVER	UP VAR VERT VC VGC WCR	VARIES VERTICAL VERTICAL CURVE VERTICAL GRANITE CURBING WHEEL CHAIR RAMP	о РМ —— он —————————————————————————————————	• РМ —— он ——	WATER GATE PARKING METER OVERHEAD CABLE/WIRE CURBING CONTOURS (ON-THE-GROUND SURVEY DATA)	-		PAVEMENT ARROW - WHITE				
ST (or EX C C S N. OSTN	EDGE OF PAVEMENT ELECTRIC TRANSFORMER EXISTING EXCAVATION FRAME AND COVER FRAME AND GRATE FOUNDATION FIELDSTONE	UP VAR VERT VC VGC	VARIES VERTICAL VERTICAL CURVE VERTICAL GRANITE CURBING	• РМ ————————————————————————————————————	• РМ —— ОН ——	WATER GATE PARKING METER OVERHEAD CABLE/WIRE CURBING	-	PROPOSED	PAVEMENT ARROW - WHITE LEGEND "ONLY" - WHITE STOP LINE				
ST (or EX	EDGE OF PAVEMENT ELECTRIC TRANSFORMER EXISTING EXCAVATION FRAME AND COVER FRAME AND GRATE FOUNDATION FIELDSTONE GARAGE	UP VAR VERT VC VGC WCR WG WIP WM	VARIES VERTICAL VERTICAL CURVE VERTICAL GRANITE CURBING WHEEL CHAIR RAMP WATER GATE WROUGHT IRON PIPE WATER METER/WATER MAIN	∘ PM — ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○	• РМ — ОН —	WATER GATE PARKING METER OVERHEAD CABLE/WIRE CURBING CONTOURS (ON-THE-GROUND SURVEY DATA) CONTOURS (PHOTOGRAMMETRIC DATA) UNDERGROUND STORM PIPE (DOUBLE LINE 24 INCH AND OVER) UNDERGROUND ELECTRIC DUCT (DOUBLE LINE 24 INCH AND OVER)	-	PROPOSED T SL	PAVEMENT ARROW - WHITE LEGEND "ONLY" - WHITE STOP LINE CROSSWALK				
ST (or EX C C C S N. OSTN R	EDGE OF PAVEMENT ELECTRIC TRANSFORMER EXISTING EXCAVATION FRAME AND COVER FRAME AND GRATE FOUNDATION FIELDSTONE	UP VAR VERT VC VGC WCR WG	VARIES VERTICAL VERTICAL CURVE VERTICAL GRANITE CURBING WHEEL CHAIR RAMP WATER GATE WROUGHT IRON PIPE	• PM ————————————————————————————————————	• РМ — он — sт — gas —	WATER GATE PARKING METER OVERHEAD CABLE/WIRE CURBING CONTOURS (ON-THE-GROUND SURVEY DATA) CONTOURS (PHOTOGRAMMETRIC DATA) UNDERGROUND STORM PIPE (DOUBLE LINE 24 INCH AND OVER) UNDERGROUND ELECTRIC DUCT (DOUBLE LINE 24 INCH AND OVER) UNDERGROUND GAS MAIN (DOUBLE LINE 24 INCH AND OVER)	EXISTING	PROPOSED The second se	PAVEMENT ARROW - WHITE LEGEND "ONLY" - WHITE STOP LINE CROSSWALK SOLID WHITE LINE - 6"				
ST (or EX C C C S N. OSTN R	EDGE OF PAVEMENT ELECTRIC TRANSFORMER EXISTING EXCAVATION FRAME AND COVER FRAME AND GRATE FOUNDATION FIELDSTONE GARAGE GROUND GAS GATE GUTTER INLET	UP VAR VERT VC VGC WCR WG WIP WM	VARIES VERTICAL VERTICAL CURVE VERTICAL GRANITE CURBING WHEEL CHAIR RAMP WATER GATE WROUGHT IRON PIPE WATER METER/WATER MAIN	∘ PM — ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○ ○	- PM - OH	WATER GATE PARKING METER OVERHEAD CABLE/WIRE CURBING CONTOURS (ON-THE-GROUND SURVEY DATA) CONTOURS (PHOTOGRAMMETRIC DATA) UNDERGROUND STORM PIPE (DOUBLE LINE 24 INCH AND OVER) UNDERGROUND ELECTRIC DUCT (DOUBLE LINE 24 INCH AND OVER)	EXISTING	PROPOSED T SL SWL SYL	PAVEMENT ARROW - WHITE LEGEND "ONLY" - WHITE STOP LINE CROSSWALK SOLID WHITE LINE - 6" SOLID YELLOW LINE - 6"				
ST (or EX	EDGE OF PAVEMENT ELECTRIC TRANSFORMER EXISTING EXCAVATION FRAME AND COVER FRAME AND GRATE FOUNDATION FIELDSTONE GARAGE GROUND GAS GATE GUTTER INLET GALVANIZED IRON PIPE	UP VAR VERT VC VGC WCR WG WIP WM	VARIES VERTICAL VERTICAL CURVE VERTICAL GRANITE CURBING WHEEL CHAIR RAMP WATER GATE WROUGHT IRON PIPE WATER METER/WATER MAIN	• PM ———————————————————————————————————	- PM - OH - OH - ST - GAS SFM S - SFM S - SFM S - SFM S - SFM - S	WATER GATE PARKING METER OVERHEAD CABLE/WIRE CURBING CONTOURS (ON-THE-GROUND SURVEY DATA) CONTOURS (PHOTOGRAMMETRIC DATA) UNDERGROUND STORM PIPE (DOUBLE LINE 24 INCH AND OVER) UNDERGROUND ELECTRIC DUCT (DOUBLE LINE 24 INCH AND OVER) UNDERGROUND GAS MAIN (DOUBLE LINE 24 INCH AND OVER) UNDERGROUND SEWER FORCE MAIN (DOUBLE LINE 24 INCH AND OVER) UNDERGROUND SEWER LINE (DOUBLE LINE 24 INCH AND OVER) UNDERGROUND WATER MAIN (DOUBLE LINE 24 INCH AND OVER)	EXISTING	PROPOSED SL CW SWL SYL BWL	PAVEMENT ARROW - WHITE LEGEND "ONLY" - WHITE STOP LINE CROSSWALK SOLID WHITE LINE - 6" SOLID YELLOW LINE - 6" BROKEN WHITE LINE - 6" (10' LINE SEGMENT AND 30' GAP)				
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TRAFFIC SIGNAL CONDUIT STEADY WALKING PERSON STEADY CIRCULAR YELLOW STEADY YELLOW LEFT ARROW



LEGEND

COMPLETE STREETS
PROJECTS
PREPARED FOR
TOWN OF HOLLISTON

GENERAL NOTES

- 1. THE WORK UNDER THIS PROJECT INCLUDES BUT IS NOT LIMITED TO THE CONSTRUCTION OF A CONCRETE SIDEWALK WITH WHEELCHAIR RAMPS AND GRANITE CURBING, PROTECTION OF EXISTING UTILITIES TO REMAIN, INSTALLATION OF TRAFFIC SIGNS, PLACEMENT OF PAVEMENT MARKINGS, GRADING, AND TRAFFIC MAINTENANCE DURING CONSTRUCTION.
- 2. THE CONTRACTOR SHALL NOTIFY THE TOWN OF ALL CONFLICTS BETWEEN THE EXISTING UTILITIES AND PROPOSED WORK. THE ENGINEER RESERVES THE RIGHT TO MODIFY THE DESIGN. ALL UTILITIES AND THEIR APPURTENANCES SHALL BE RETAINED AND PROTECTED UNLESS OTHERWISE NOTED.
- 3. ALL EXISTING STREET NAME, REGULATORY, GUIDE, AND WARNING SIGNS, INCLUDING POSTS, WITHIN THE LIMITS OF WORK ARE TO BE RETAINED UNLESS OTHERWISE NOTED.
- 4. THE CONTRACTOR SHALL RETAIN AND PROTECT ALL CURBS, FENCES, WALLS, TREES, SHRUBS, POSTS, LANDSCAPE FEATURES, AND OTHER MISCELLANEOUS ITEMS WITHIN ABUTTING PROPERTIES UNLESS OTHERWISE NOTED. WHEN RETAINING THOSE ITEMS IS NOT PRACTICAL IN THE OPINION OF THE ENGINEER, THE CONTRACTOR SHALL REMOVE, STOCKPILE, PROTECT AND RESET THE ITEMS. THE CONTRACTOR SHALL REPLACE ITEMS DAMAGED DURING REMOVAL, STOCKPILING, OR RESETTING DUE TO NEGLIGENCE, CARELESSNESS, OR MISHANDLING WITH EQUIVALENT NEW ITEMS AT NO COST TO THE OWNER. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING ALL EXISTING TREES NOT CALLED OUT IN THE PLANS TO BE REMOVED. THE COSTS FOR PROTECTION SHALL BE INCIDENTAL TO THE CONTRACT.
- 5. THE CONTRACTOR SHALL PROVIDE ALL SAFETY CONTROL (SIGNS, REFLECTORIZED DRUMS, ETC) FOR CONSTRUCTION OPERATIONS IN ACCORDANCE WITH THE TRAFFIC MANAGEMENT PLANS INCLUDED HEREIN AND THE 2009 MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- 6. ALL MATERIALS TO BE REMOVED AND STACKED SHALL BE DELIVERED BY THE CONTRACTOR TO THE TOWN OF HOLLISTON AND STACKED AT THE HOLLISTON HIGHWAY DEPARTMENT, 63 ARCH STREET.
- 7. THE CONTRACTOR SHALL INSTALL ALL EROSION CONTROL MEASURES PRIOR TO THE START OF WORK.
- 8. ALL UTILITIES AND THEIR APPURTENANCES SHALL BE RETAINED AND PROTECTED UNLESS OTHERWISE NOTED.
- 9. THE CONTRACTOR SHALL NOT WORK BEYOND THE LINES SHOWN ON THE PLANS AS PROPOSED SLOPE LIMITS, UNLESS APPROVED BY THE ENGINEER.
- 10. THE CONTRACTOR SHALL TRANSITION THE CURB TO MATCH THE EXISTING CONDITIONS AT THE LIMITS OF WORK WHERE NEW CURB IS PROPOSED OR EXISTING CURB IS RESET.
- 11. ALL CURB, CURB INLET AND CURB CORNER REMOVAL SHOULD BE INCLUDED UNDER ITEM 594.
- 12. ALL EXISTING STRAIGHT GRANITE CURB WITHIN THE PROJECT LIMITS SHALL BE REMOVED AND RESET UNLESS OTHERWISE SPECIFIED ON THE PLANS. ALL EXISTING CURVED GRANITE CURB WITHIN THE PROJECT LIMITS SHALL BE REMOVED TO DPW STORAGE AND NOT DISCARDED.
- 13. PROPOSED GRANITE CURB PLACED WITHIN THE PROJECT LIMITS SHALL BE GRANITE CURB, TYPE VB UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- 14. EXISTING SIDEWALKS AT THE FACE OF BUILDINGS THAT ARE PROPOSED TO BE RECONSTRUCTED SHALL BE SAWCUT A MINIMUM OF 6 INCHES FROM THE FACE OF THE BUILDING AND SHALL BE REMOVED BY HAND. THE CONTRACTOR SHALL CLEAN THE FACE OF BUILDING PRIOR TO INSTALLATION OF NEW SIDEWALK. ANY COSTS ASSOCIATED WITH THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CORRESPONDING SIDEWALK PAY ITEMS.

UTILITY NOTES

- 1. WHERE AN EXISTING UTILITY IS FOUND TO CONFLICT WITH THE PROPOSED WORK, THE LOCATION, ELEVATION, AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED WITHOUT DELAY BY THE CONTRACTOR AND THE INFORMATION FURNISHED TO THE ENGINEER FOR RESOLUTION OF THE CONFLICT.
- 2. THE CONTRACTOR SHALL MAKE ALL ARRANGEMENTS FOR THE ALTERATION AND ADJUSTMENT OF ELECTRIC, TELEPHONE, AND ANY OTHER PRIVATE UTILITIES BY THE UTILITY COMPANIES AT NO ADDITIONAL COST TO THE OWNER AND THE TOWN. IF THE CONTRACTOR ADJUSTS UTILITY COVERS IT SHALL BE DEEMED PART OF THE WORK AND THERE WILL BE NO ADDITIONAL COST.
- 3. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE NECESSITY OF MAKING HIS OWN INVESTIGATION IN ORDER TO ASSURE THAT NO DAMAGE TO EXISTING STRUCTURES, DRAINAGE LINES, TRAFFIC SIGNAL CONDUITS, ETC. WILL OCCUR.
- 4. THE CONTRACTOR SHALL NOTIFY MASSACHUSETTS DIG SAFE AND PROCURE A DIG SAFE NUMBER FOR EACH LOCATION PRIOR TO DISTURBING EXISTING GROUND IN ANY WAY. THE TELEPHONE NUMBER OF THE DIG SAFE CALL CENTER IS 1-888-DIG-SAFE (1-888-344-7233).
- 5. NO EXISTING PUBLIC UTILITY STRUCTURES SHALL BE ABANDONED AND/OR DISMANTLED WITHOUT AUTHORIZATION FROM THE ENGINEER
- 6. DRAINAGE ELEVATIONS ARE PROVIDED FOR DESIGN PURPOSE ONLY. THE CONTRACTOR SHALL VERIFY BY TEST PIT, THE LOCATIONS OF EXISTING UTILITIES WHICH MAY CONFLICT WITH THE PROPOSED DRAINAGE DESIGN. ANY FIELD ADJUSTMENTS REQUIRED WILL BE MADE AS APPROVED OR DIRECTED BY THE ENGINEER. ONLY AFTER THE CONTRACTOR VERIFIES ELEVATIONS FOR THE CONSTRUCTABILITY OF THE DRAINAGE SYSTEM SHALL ANY STRUCTURES BE ORDERED. ROCK EXCAVATION, IF NECESSARY, SHALL BE CONSIDERED INCIDENTAL TO THE INSTALLATION OF THE DRAINAGE PIPES AND STRUCTURES.

CONSTRUCTION NOTES

- AREAS OUTSIDE THE LIMITS OF PROPOSED WORK DAMAGED BY THE CONTRACTOR'S OPERATIONS, INCLUDING STAGING AREAS, SHALL BE RESTORED BY THE CONTRACTOR TO THEIR ORIGINAL CONDITION AT NO ADDITIONAL CHARGE.
- 2. THE CONTRACTOR IS HEREBY NOTIFIED THAT ADDITIONAL WORK WITHIN THE PROJECT LIMITS MAY BE PERFORMED BY OTHERS. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO COORDINATE WITH ANY SUCH WORK. NO ADDITIONAL COMPENSATION WILL BE MADE FOR EXTRA WORK DAYS, DELAYS, OR RESCHEDULING OF WORK TO ACCOMMODATE ANY OTHER CONSTRUCTION, PERMIT AND/OR MAINTENANCE OPERATIONS IN THE AREA.
- 3. JOINTS BETWEEN NEW HOT MIX ASPHALT, ROADWAY PAVEMENT, AND THE LOCATIONS OF SAW CUT FOR EXISTING PAVEMENT SHALL BE SEALED WITH HMA JOINT SEALANT AND BACKSANDED.
- 4. ALL GRADING SHALL COMPLY WITH THE RULES AND REGULATIONS OF THE MASSACHUSETTS ARCHITECTURAL ACCESS BOARD (MAAB) AND THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES LATEST EDITION. IN CASE OF CONFLICT BETWEEN REGULATIONS, THE GUIDELINE PROVIDING GREATER ACCESS SHALL APPLY. WHEELCHAIR RAMP INSTALLATIONS SHALL BE IN ACCORDANCE WITH THE MASSDOT WHEELCHAIR RAMP STANDARDS—LATEST EDITION AND THE PLANS.
- 5. WHERE THE NEW CONSTRUCTION IS WITHIN THE EXISTING TRAVELED WAY, THE CONTRACTOR SHALL PERFORM WORK SO THAT INTERFERENCE TO BUSINESS CONCERNS AND ABUTTERS, ON ACCOUNT OF THE CONSTRUCTION WORK, IS KEPT TO A MINIMUM. THE CONTRACTOR WILL NOT BE ALLOWED TO PARK EQUIPMENT, OR STOCKPILE MATERIAL ON THE TRAVELED WAYS OVERNIGHT OR WHEN NOT IN USE. THE CONTRACTOR SHALL MAINTAIN SAFE AND REASONABLE ACCESS TO AND FROM ABUTTING PROPERTIES AT ALL TIMES WITH NO ADDITIONAL COMPENSATION.
- 6. THE CONTRACTOR SHALL DISPOSE OF ALL WASTE MATERIAL IN ACCORDANCE WITH ALL FEDERAL, STATE AND LOCAL REGULATIONS AT HIS OWN EXPENSE, OUTSIDE OF THE PROJECT LIMITS.
- 7. CONTRACTOR SHALL BE RESPONSIBLE FOR INVESTIGATING AND CONFIRMING THAT ALL ITEMS TO BE REUSED ARE IN SERVICEABLE CONDITION. IF IT IS DEEMED THAT ANY ITEM IS NOT ABLE TO BE REUSED, THE CONTRACTOR SHALL NOTIFY THE TOWN IN WRITING AND INCLUDE ESTIMATED COSTS TO REPLACE.

TEMPORARY TRAFFIC CONTROL

- 1. THE TEMPORARY TRAFFIC CONTROL PLANS DEPICT IN SCHEMATIC FORM, THE ELEMENTS OF AN APPROACH TO THE LAYOUT AND PLANNING OF THE WORK DURING THE PROGRESS OF THE CONSTRUCTION OPERATIONS. THE PREPARER OF THESE PLANS HAS NO ROLE IN THE OVERSIGHT OR OTHERWISE IN THE IMPLEMENTATION OF THESE PLANS.
- 2. CONTRACTOR SHALL SUBMIT TO THE TOWN AND/OR ITS ENGINEER TRAFFIC MANAGEMENT PLANS FOR REVIEW AND APPROVAL BY THE RESIDENT ENGINEER. CONTRACTOR SHALL COORDINATE THE CONSTRUCTION EFFORT WITH OTHER PROJECTS IN THE VICINITY IN ORDER TO MINIMIZE POTENTIAL TRAFFIC IMPACTS
- 3. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE SHOP DRAWINGS FOR TRAFFIC MANAGEMENT AND TO COMPLY WITH CONDITIONS
 OUTLINED WITHIN THE SPECIFICATIONS AND MASSDOT HIGHWAY DIVISION STANDARD DETAILS AND DRAWINGS FOR THE DEVELOPMENT OF TRAFFIC MANAGEMENT PLANS MANUAL
- 4. THE TEMPORARY TRAFFIC CONTROL PLANS CONTAINED HEREIN ARE GIVEN AS A GUIDE FOR TYPICAL WORK ZONE TRAFFIC CONTROL APPLICATIONS FOR THE TYPES OF WORK ANTICIPATED FOR THIS PROJECT. THEY ARE NOT INTENDED TO COVER ALL POSSIBLE CONSTRUCTION OPERATIONS WHICH THE CONTRACTOR MAY CHOOSE TO WORK ZONE TRAFFIC CONTROL FOR OTHER CONSTRUCTION OPERATIONS OR OTHER TRAFFIC SITUATIONS IF APPLICABLE SHALL BE IN ACCORDANCE WITH THE CURRENT M.U.T.C.D. AND AS APPROVED OR DIRECTED BY RESIDENT ENGINEER.
- 5. LANE RESTRICTIONS (OTHER THAN ACTIVE WORK ZONES) MAY NOT REMAIN OVERNIGHT OR DURING NON-WORKING HOURS AND MUST BE REMOVED BY THE END OF EACH WORKING TIME RESTRICTION. AFTER EACH WORKING DAY, TRAFFIC CONTROL DEVICES THAT ARE NOT REQUIRED SHALL BE MOVED OFF THE ROADWAY OR FULL DEPTH CONSTRUCTION AREA AND PLACED SO AS NOT TO IMPEDE PEDESTRIAN AREAS, ABUTTER ACCESS OR CAUSE CONFUSION TO ROADWAY USERS. IN CERTAIN CIRCUMSTANCES, AND ONLY WITH THE APPROVAL OF TOWN OF SHERBORN, CAN LANE RESTRICTIONS REMAIN OVERNIGHT. REFLECTORIZED DRUMS MAY BE FITTED WITH STEADY BURN AND/OR FLASHING WARNING LIGHTS AT ONLY THE RESIDENT ENGINEERS DIRECTION.
- 6. PLACE ALL CONSTRUCTION SIGNING, TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENTS MARKINGS FOR EACH PHASE PRIOR TO COMMENCEMENT OF CONSTRUCTION
- 7. THESE PLANS ARE NOT INTENDED TO LIMIT THE CONTRACTORS RIGHT TO SCHEDULE THE WORK BUT TO OUTLINE ONE WAY OF PROGRESSING. THE CONTRACTOR IS EXPECTED TO USE KNOWLEDGE AND EXPERIENCE TO PERFORM THE WORK IN THE MOST EFFICIENT MANNER IN COMPLIANCE WITH THE DRAWING AND SPECIFICATIONS AND THE REQUIREMENTS OF THE INDIVIDUAL AGENCIES AND ABUTTERS.
- 8. CONTRACTOR SHALL SECURE WORK AREAS ACCORDING TO CURRENT CONDITIONS TO ENSURE PUBLIC SAFETY AND CONVENIENCE. THIS SHALL INCLUDE ENSURING THAT ALL EXCAVATIONS ARE PROTECTED AT ALL TIMES AND WHEN WORK SHIFT IS COMPLETED.
- 9. CONTRACTOR SHALL SUBMIT FOR REVIEW AND APPROVAL BY THE TOWN AND/OR ITS ENGINEER AND THE DESIGNER, TEMPORARY TRAFFIC CONTROL PLANS FOR ANY WORK OUTSIDE THE WORK ZONES INDICATED IN THESE DRAWINGS, INCLUDING ALTERNATIVE PHASING OR MODIFICATION OF ANY ASPECT OF THE TEMPORARY TRAFFIC CONTROL PLANS OR CONSTRUCTION STAGING. THE CONTRACTOR SHALL BEAR RESPONSIBILITY FOR THE SUBMISSION AND REVIEW OF ALTERNATIVE PLANS, AT NO ADDITIONAL COST.
- 10. EXISTING CONDITIONS ARE FOR CONTRACTOR INFORMATION ONLY AND ARE EXISTING CONDITIONS AT THE TIME OF DESIGN. THE CONTRACTOR SHALL VERIFY, AS NECESSARY, ACTUAL FIELD CONDITIONS AT TIME OF CONSTRUCTION.

- 11. PLACE ALL CONSTRUCTION SIGNING, TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENTS MARKINGS FOR EACH PHASE PRIOR TO COMMENCEMENT OF CONSTRUCTION
 THESE PLANS ARE NOT INTENDED TO LIMIT THE CONTRACTORS RIGHT TO SCHEDULE THE WORK BUT TO OUTLINE ONE WAY OF PROGRESSING. THE CONTRACTOR IS EXPECTED
- 12. TYPICAL DAYTIME WORK HOURS ARE FROM 9:00 AM TO 3:30 PM ON WEEKDAYS, UNLESS OTHERWISE PERMITTED BY THE TOWN. WORK SHALL NOT BE PERFORMED THE DAY BEFORE, OR THE DAY AFTER, A HOLIDAY WEEKEND, UNLESS OTHERWISE PERMITTED BY THE TOWN. REFER TO TEMPORARY TRAFFIC CONTROL PLANS, SPECIFICATIONS, AND PERMITS FOR MODIFICATION TO ALLOWABLE WORK PERIODS. ALL WORK SCHEDULES, HOWEVER, SHALL BE PRE—APPROVED BY THE TOWN PRIOR TO BEGINNING WORK. WORK NECESSARY OUTSIDE OF THESE NORMAL WORK HOURS BECAUSE OF TRAFFIC CONDITIONS, AS NOTED IN THE PLANS OR SPECIFICATIONS, SHALL BE APPROVED BY THE TOWN AND/OR ITS ENGINEER.
- 13. WORK ZONES INDICATED ON THE TEMPORARY TRAFFIC CONTROL PLANS ARE INTENDED FOR THE DURATION OF THE WORK WITHIN THE ZONES ONLY AND SHALL BE RESTORED TO CONDITIONS ACCEPTABLE TO THE TOWN AT COMPLETION OF THE WORK INDICATED.
- 14. CONTRACTOR SHALL COORDINATE WITH THE TOWN CONCERNING ALL SCHEDULED SPECIAL EVENTS WITHIN THE LIMITS OF WORK
- 15. SIGNS AND SIGN SUPPORTS LOCATED ON OR NEAR THE TRAVELED WAY, CHANNELIZING DEVICES, BARRIERS, AND CRASH ATTENUATORS MUST PASS THE CRITERIA SET FORTH IN THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
- 16. CONTRACTORS SHALL NOTIFY EACH ABUTTER AT LEAST 24 HOURS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE THE TEMPORARY CLOSURE OF ACCESS, SUCH AS CONDUIT INSTALLATION, EXISTING PAVEMENT EXCAVATION, TEMPORARY DRIVEWAY PAVEMENT PLACEMENT, AND SIMILAR OPERATIONS.
- 17. DISTANCES ARE A GUIDE AND MAY BE ADJUSTED IN THE FIELD BY THE ENGINEER.
- 18. MAXIMUM SPACING OF TRAFFIC DEVICES IN A TAPER (DRUMS OR CONES) IS EQUAL IN FEET TO THE SPEED LIMIT IN MPH.
- 19. MINIMUM LANE WIDTHS TO BE 11 FEET UNLESS OTHERWISE SHOWN. MINIMUM LANE WIDTH TO BE MEASURED FROM THE EDGE OF DRUM OR MEDIAN BARRIER.

CHANNELIZATION

- 1. CHANNELIZATION SHALL BE ACCOMPLISHED THROUGH THE USE OF REFLECTORIZED PLASTIC DRUMS IN ACCORDANCE WITH THE CURRENT M.U.T.C.D. ALL LANE TAPERS SHALL BE IN ACCORDANCE WITH THE CURRENT M.U.T.C.D.
- 2. ALL DRUMS SHALL BE PLACED AND MOVED AS NECESSARY TO MAINTAIN ADEQUATE ABUTTER ACCESS AT ALL TIMES. WORK MAY REQUIRE ADDITIONAL SIGNS, DRUMS, AND OTHER TRAFFIC CONTROL DEVICES.
- 3. THE MAXIMUM SPACING BETWEEN CHANNELIZATION DEVICES (DRUMS OR CONES) SHALL BE APPROXIMATELY EQUAL IN FEET TO THE POSTED SPEED LIMIT.
- 4. THE FIRST FIVE PLASTIC DRUMS OF A TAPER SHALL BE MOUNTED WITH SEQUENTIAL FLASHING LIGHTS.
- 5. METAL DRUMS ARE PROHIBITED AS CHANNELIZATION DEVICES.

CONSTRUCTION SIGNING

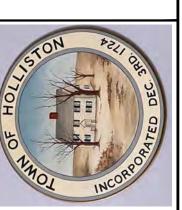
- 1. LOCATIONS OF SIGNS SHOWN ARE APPROXIMATE. EXACT LOCATION SHALL BE DETERMINED BY THE CONTRACTOR IN THE FIELD. THE CONTRACTOR SHALL ENSURE THAT SIGNS ARE PLACED IN ACCORDANCE WITH THE CURRENT M.U.T.C.D.
- 2. EXISTING SIGNING WHICH CONFLICTS WITH PROPOSED CONSTRUCTION TRAFFIC MANAGEMENT SIGNING SHALL BE REMOVED AND STACKED OR COVERED AND RESTORED AT THE END OF THE WORK.
- 3. ALL SIGNS SHALL BE COVERED OR REMOVED WHEN CONDITION IS NOT IN EFFECT.

PAVEMENT MARKINGS

- 1. UNLESS OTHERWISE NOTED, ALL PAVEMENT MARKINGS, SIGNS AND OTHER TRAFFIC EQUIPMENT REMOVED OR DAMAGED AS A RESULT OF THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED IN ACCORDANCE WITH THE REQUIREMENTS OF THE MUTCD.
- 2. CONTRACTOR SHALL INSTALL, RENEW AND MAINTAIN ALL TRAFFIC CONTROL DEVICES INCLUDING PAVEMENT MARKINGS AS SHOWN ON THE DRAWINGS, IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND AS REQUIRED BY THE RESIDENT ENGINEER.
- 3. CONTRACTOR SHALL REMOVE ALL PAVEMENT MARKINGS WHICH CONFLICT WITH PROPOSED PAVEMENT MARKINGS. THE METHOD OF REMOVAL SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE TOWN.
- 4. ALL TEMPORARY PAVEMENT MARKINGS SHALL BE MAINTAINED THROUGHOUT THE ENTIRE SEQUENCE. ALL EXISTING MARKING WITHIN THE PROJECT LIMITS SHALL BE REMOVED AND REPLACED AS INDICATED ON THE PAVEMENT MARKING PLANS



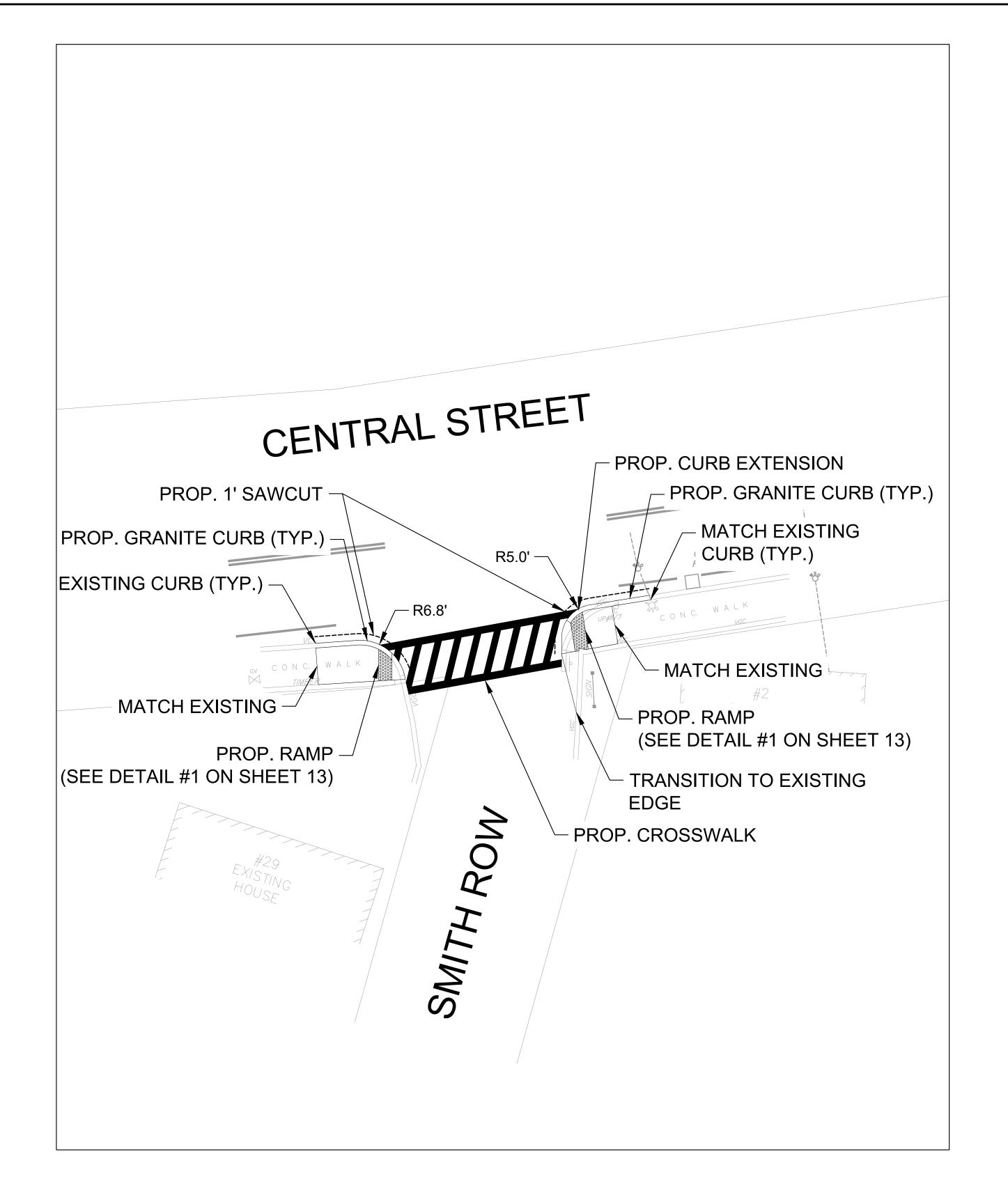
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YMAN STREET, SUITE 385, WALTHAM, MA 02451
PHONE: 781-328-0676
WWW.KIMLEY-HORN.COM



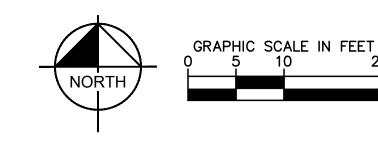
DATE
09/29/2023
SCALE AS SHOWN
DESIGNED BY WJS
DRAWN BY PAG

ENERAL NOTES

ROJECTS
REPARED FOR
OF HOLLISTON







No. REVISIONS

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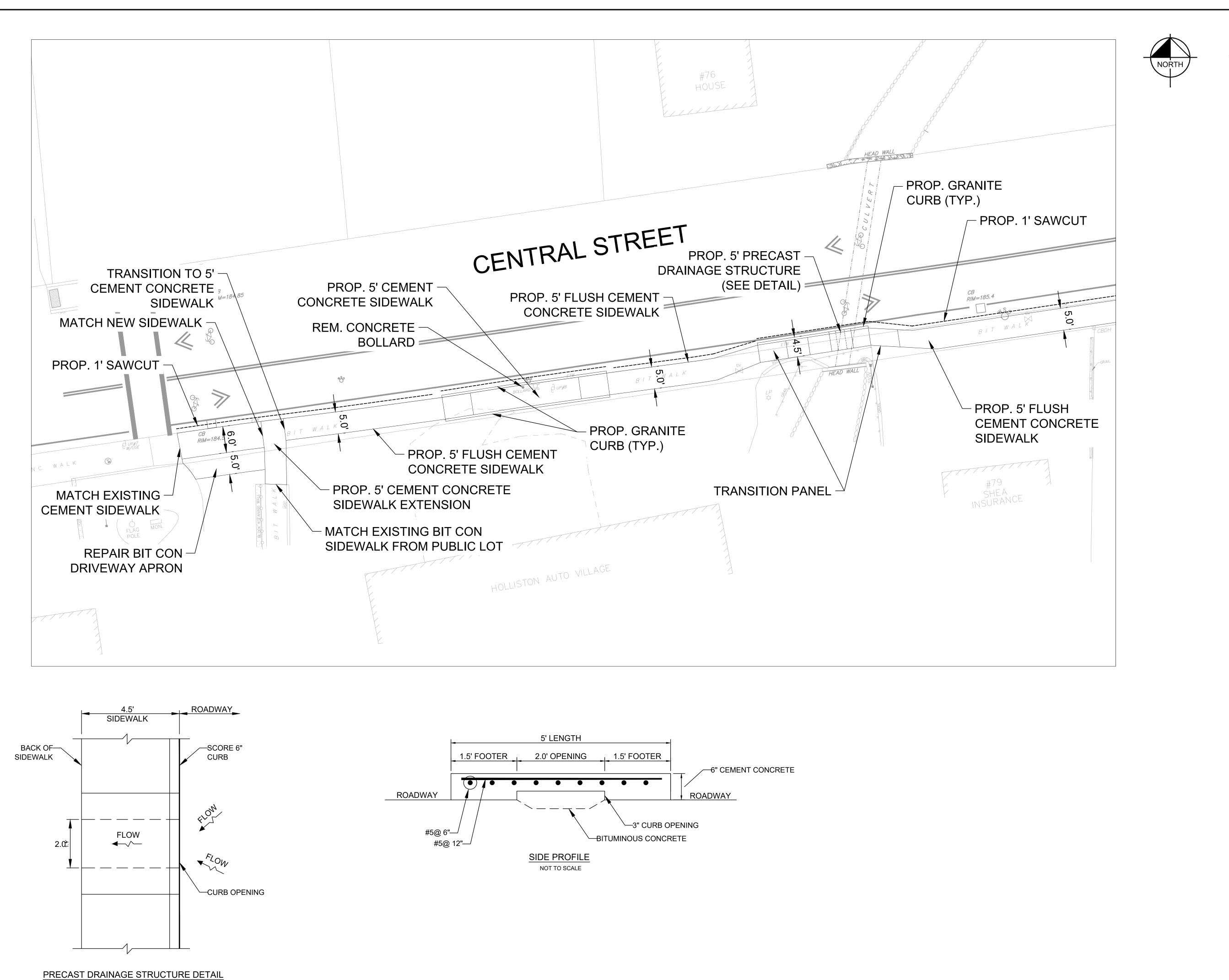
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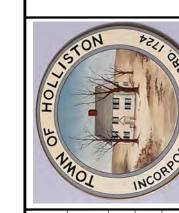
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NSTRUCTION PLAN

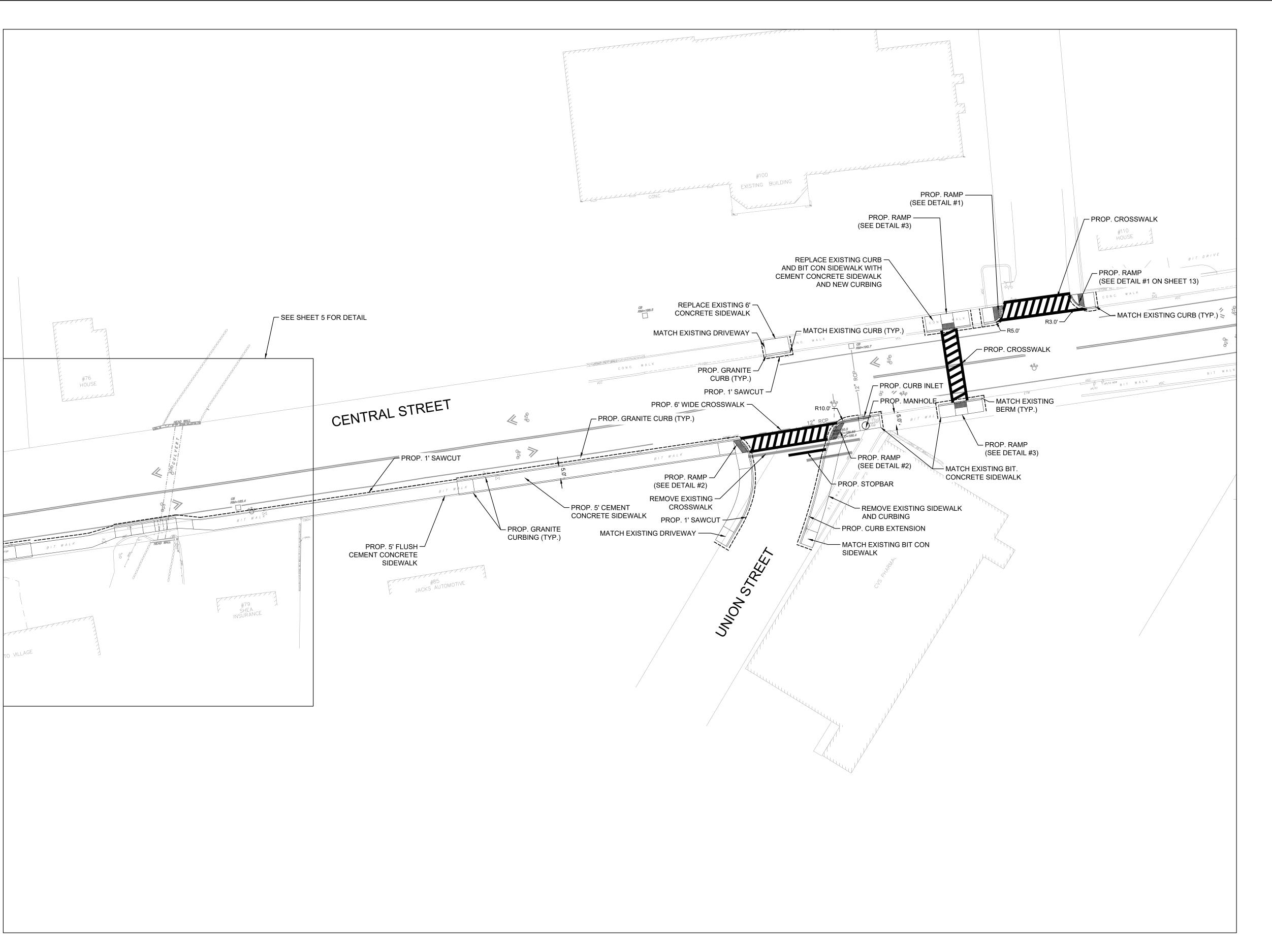
PROJECTS
PREPARED FOR
N OF HOLLISTON



NOT TO SCALE



CONSTRUCTION PLANS

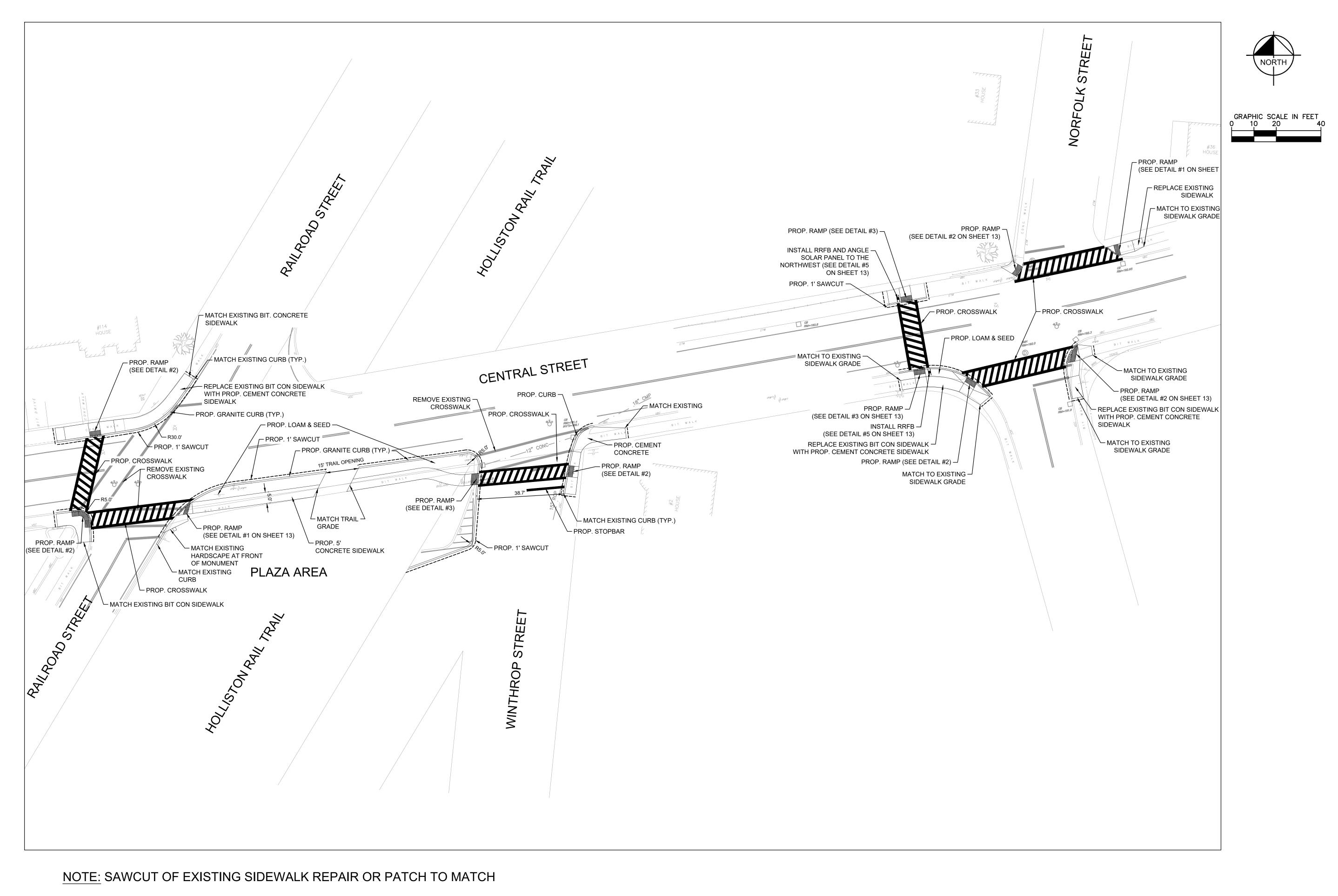


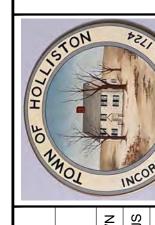


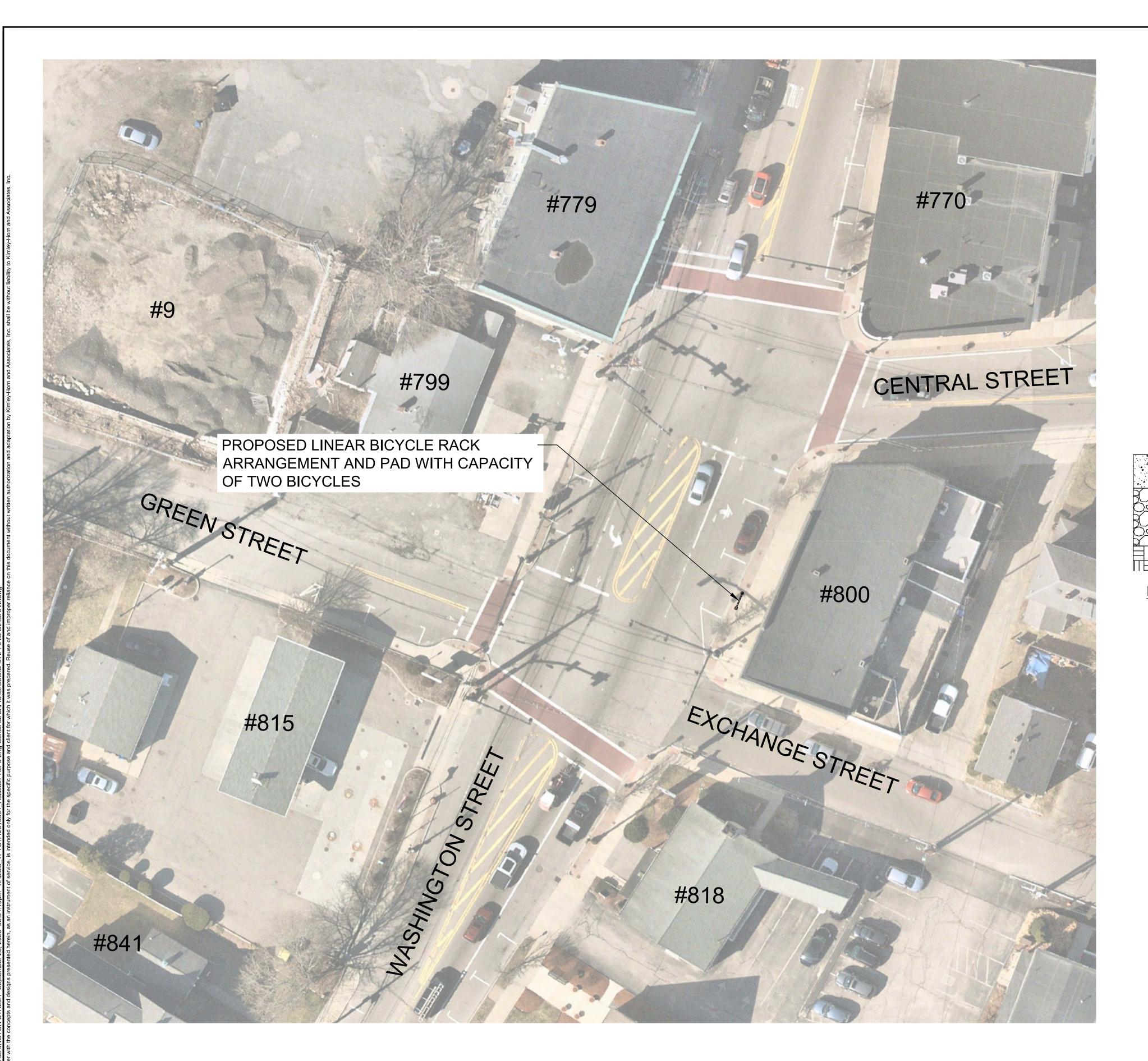
CONSTRUCTION PLANS

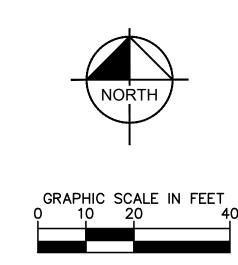
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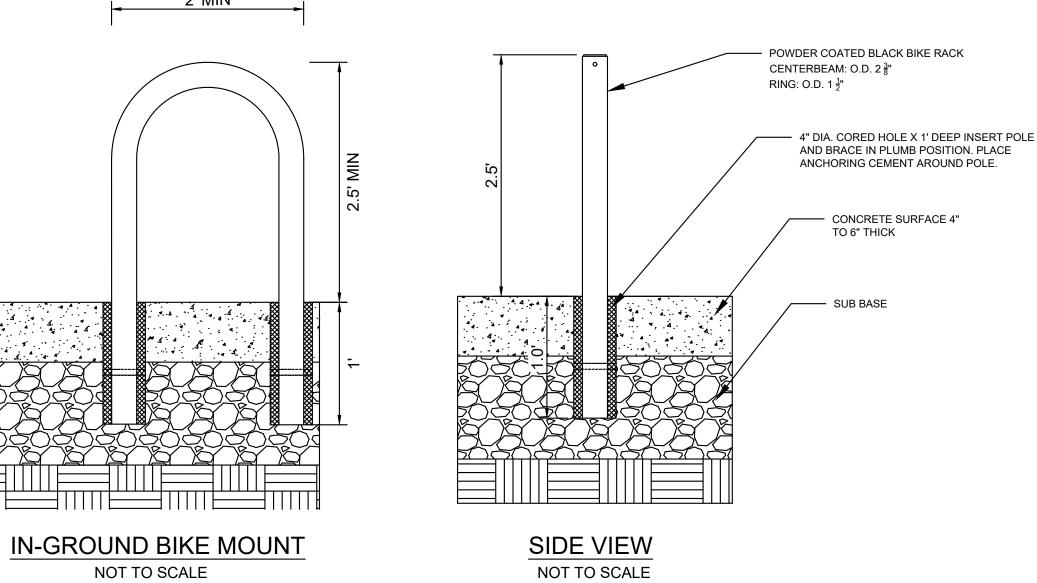
NOTE: SAWCUT OF EXISTING SIDEWALK REPAIR OR PATCH TO MATCH











NOT TO SCALE



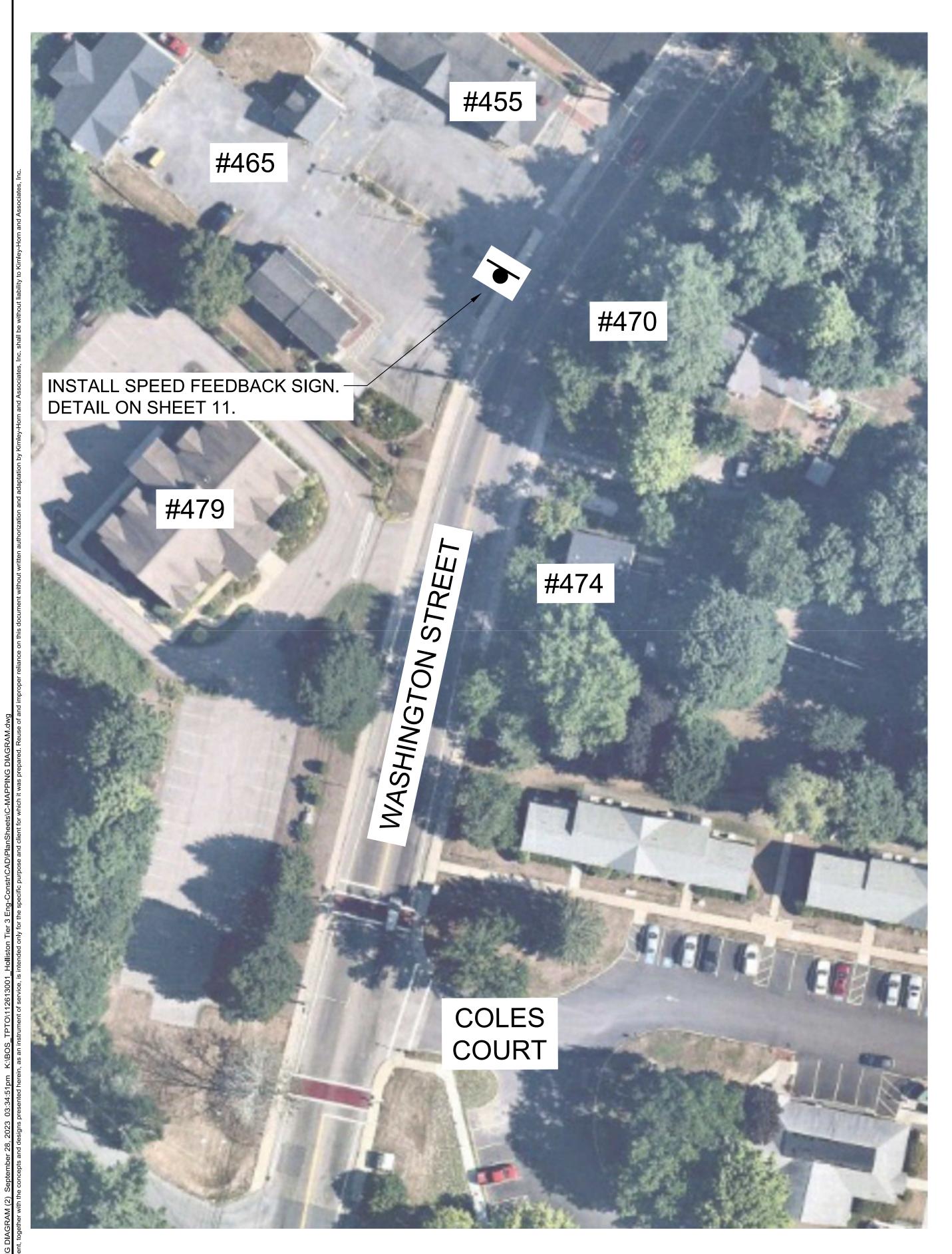
RACK - WASHING STREET

COMPLETE STREETS
PROJECTS

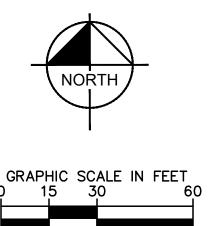
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NOTES

- 1. BICYCLE RACKS SHALL BE INSTALLED AT LEAST 3 FEET FROM ANY OBSTRUCTIONS AND AT LEASE 2 FEET FROM THE BACK OF CURB, AND ORIENTED PARALLEL TO THE ROADWAY.
- 2. CORE HOLES FOR CONCRETE FOOTINGS SHALL BE NO LESS THAN 4 INCHES IN DIAMETER AND NO LESS THAN 12 INCHES DEEP INTO THE GROUND.





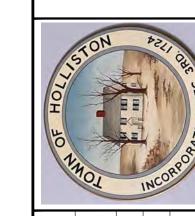


NOTES

1. ALL SIGN LOCATIONS SHALL BE VERIFIED BY THE TOWN OF HOLLISTON, THE ENGINEER, AND THE CONTRACTOR PRIOR TO INSTALLATION.

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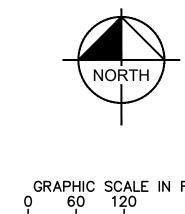
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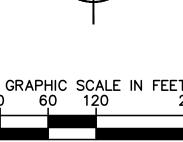


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SCALE AS SHOWN
DESIGNED BY WJS

SPEED FEEDBACK
USPLAY TWO LOCATION

PROJECTS
PREPARED FOR
VN OF HOLLISTON



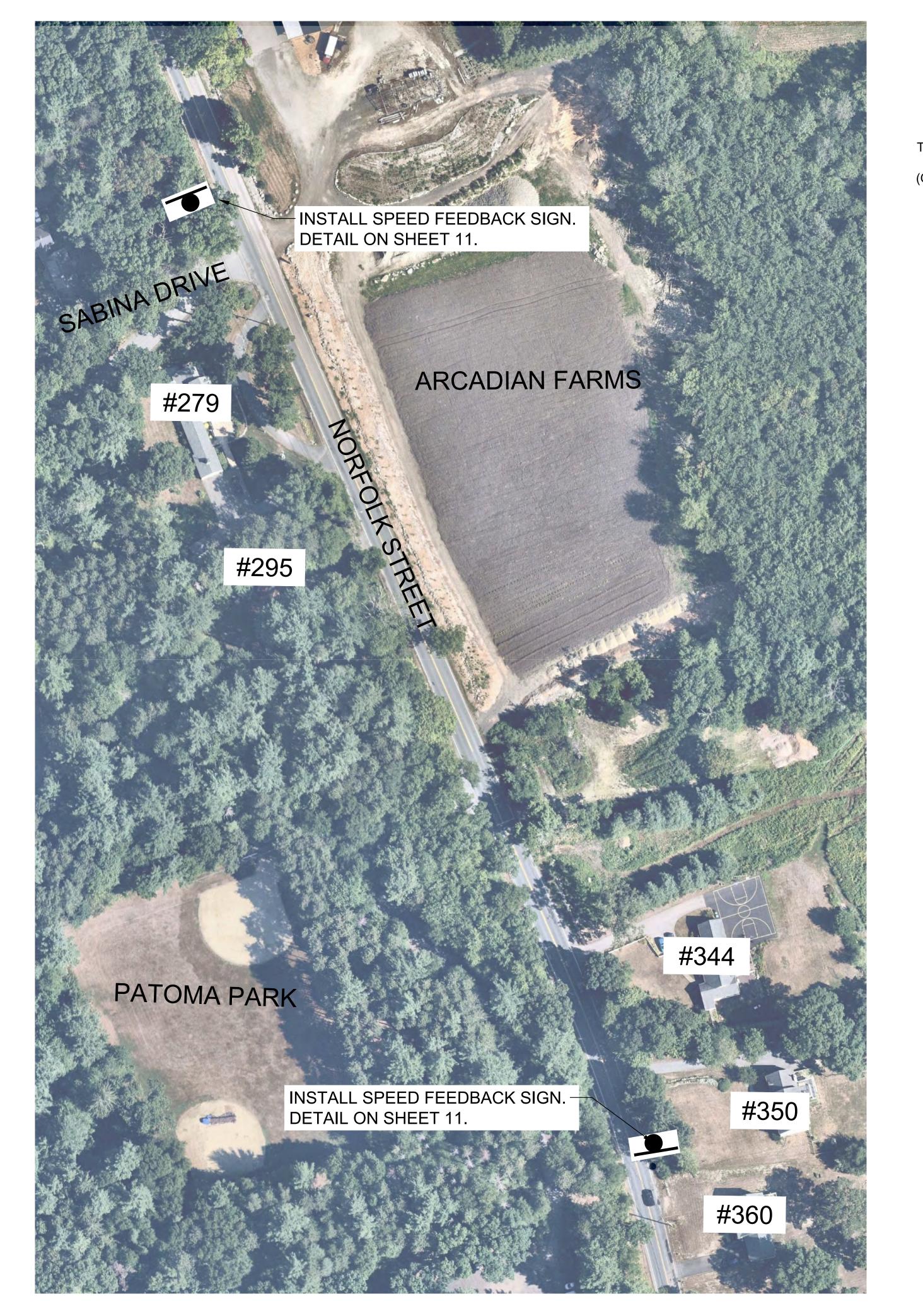


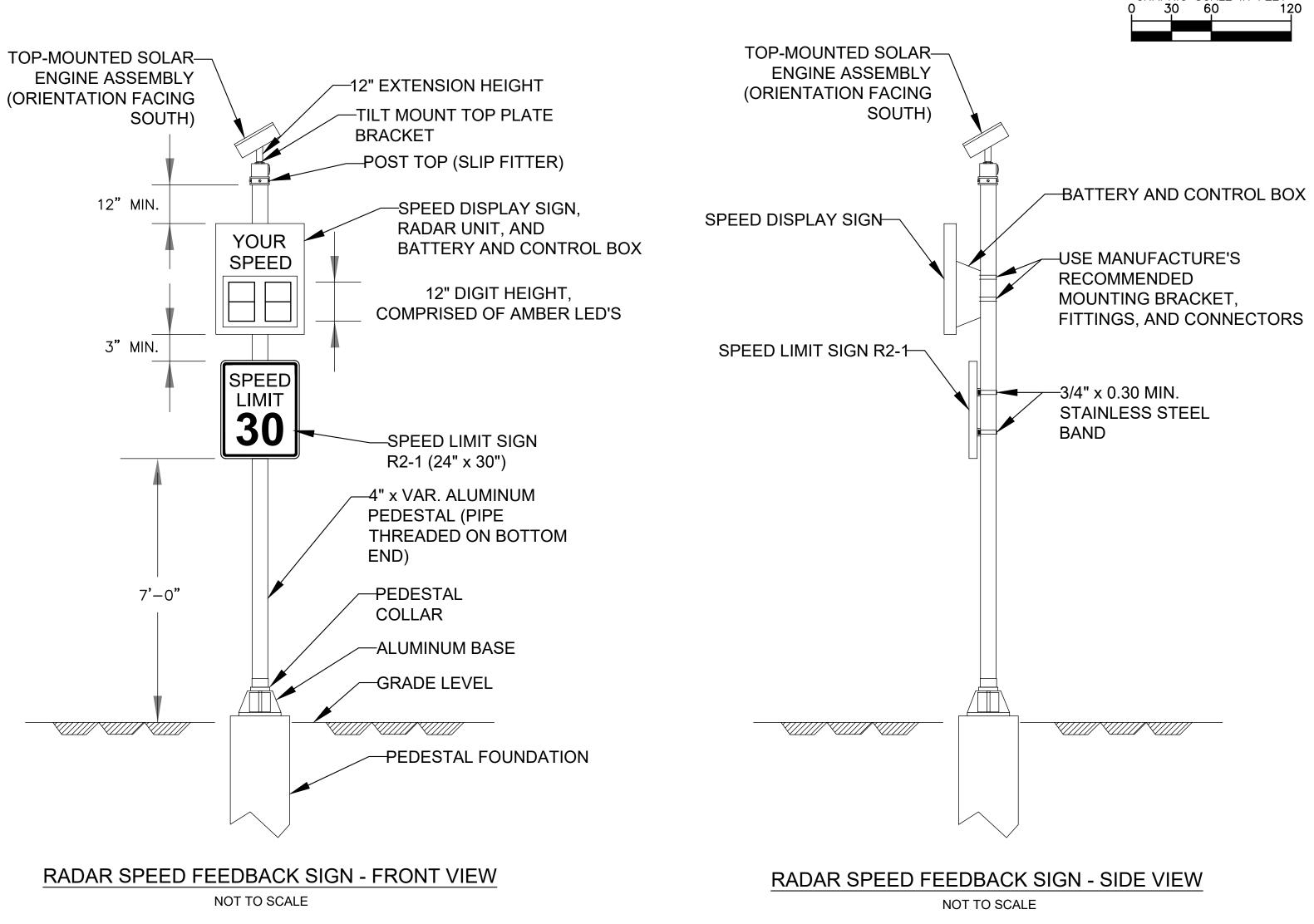


NOTES

1. ALL SIGN LOCATIONS SHALL BE VERIFIED BY THE TOWN OF HOLLISTON, THE ENGINEER, AND THE CONTRACTOR PRIOR TO INSTALLATION.







NOTES

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SPEED FEEDBACK DISPLAYS NORFOLI

PROJECTS
PREPARED FOR
TOWN OF HOLLISTON

PERMANENT SIGNS

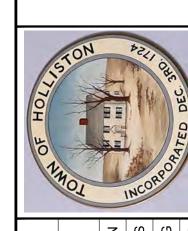
IDENTIFI- CATION NUMBER	SIZE OF SIGN			TEXT DI	MENSIONS (INCHES)	NUMBER OF SIGNS	COI	POST SIZE AND		UNIT AREA	AREA IN SQUARE		
	WIDTH	HEIGHT	TEXT	LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.	REQUIRED	BACK- GROUND LEGEND		RDER	NUMBER REQUIRED		(S.F.)	FEET
W11-2	30	30		SEE MUTC	D STANDAF	RD DETAIL	4	SEE MUTCD STA	NDARD [DETAIL		OT SPEC. IT ON OST	6.25	24
W16-7PR	24	12					4						2	8
W16-7PL	24	12				•	4	V		V			2	8

TEMPORARY SIGNS

IDENTIFI- CATION NUMBER	SIZE OF SIGN			TE	EXT DIN	MENSIONS	NUMBER OF SIGNS	COLOR					POST SIZE AND NUMBER REQUIRED		UNIT AREA (S.F.)	AREA IN SQUARE FEET	
	WIDTH	HEIGHT	TEXT			VERTICAL ARRO RTE. M		REQUIRED									
W5-1	30	30	ROAD	SEE	MUTC) STANDA	RD DETAIL	1	SEE	Т МИТО	D ST	ANDARD DE	TAIL	MASSDO MOUN PO	OT SPEC. NT ON OST	6.25	6.25
W13-1P	18	18	XX M.P.H.					2								2.25	4.50
W20-4	30	30	ONE LANE ROAD XXX					2								6.25	12.50
W20-7	30	30						2								6.25	12.50
W20-7B	30	30	POLICE OFFICER AHEAD					2								6.25	12.50
W21-5A	30	30	RIGHT SHOULDER CLOSED	,		V		1	,	Y			V			6.25	6.25

No. REVISIONS DATE





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DATE

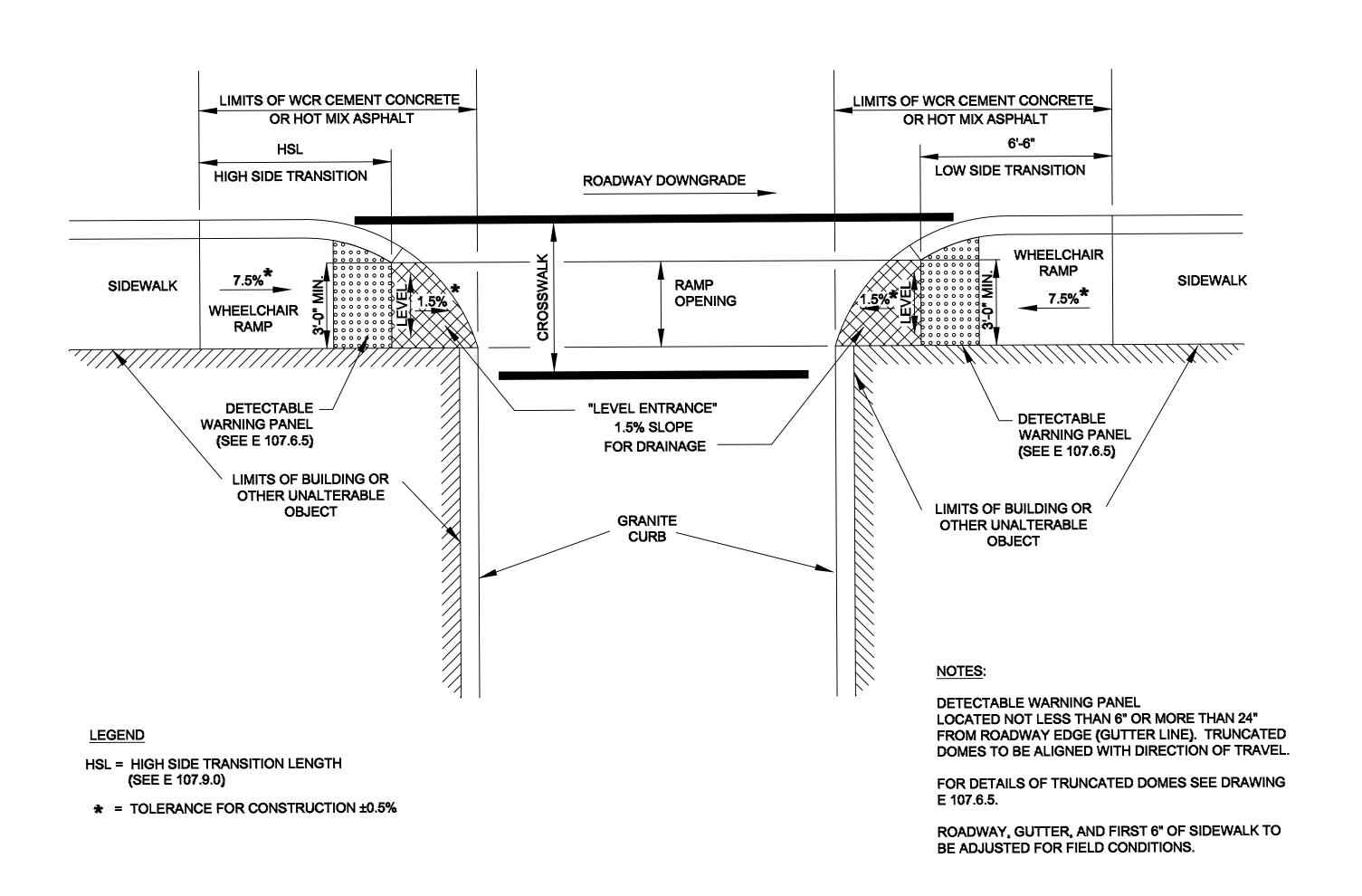
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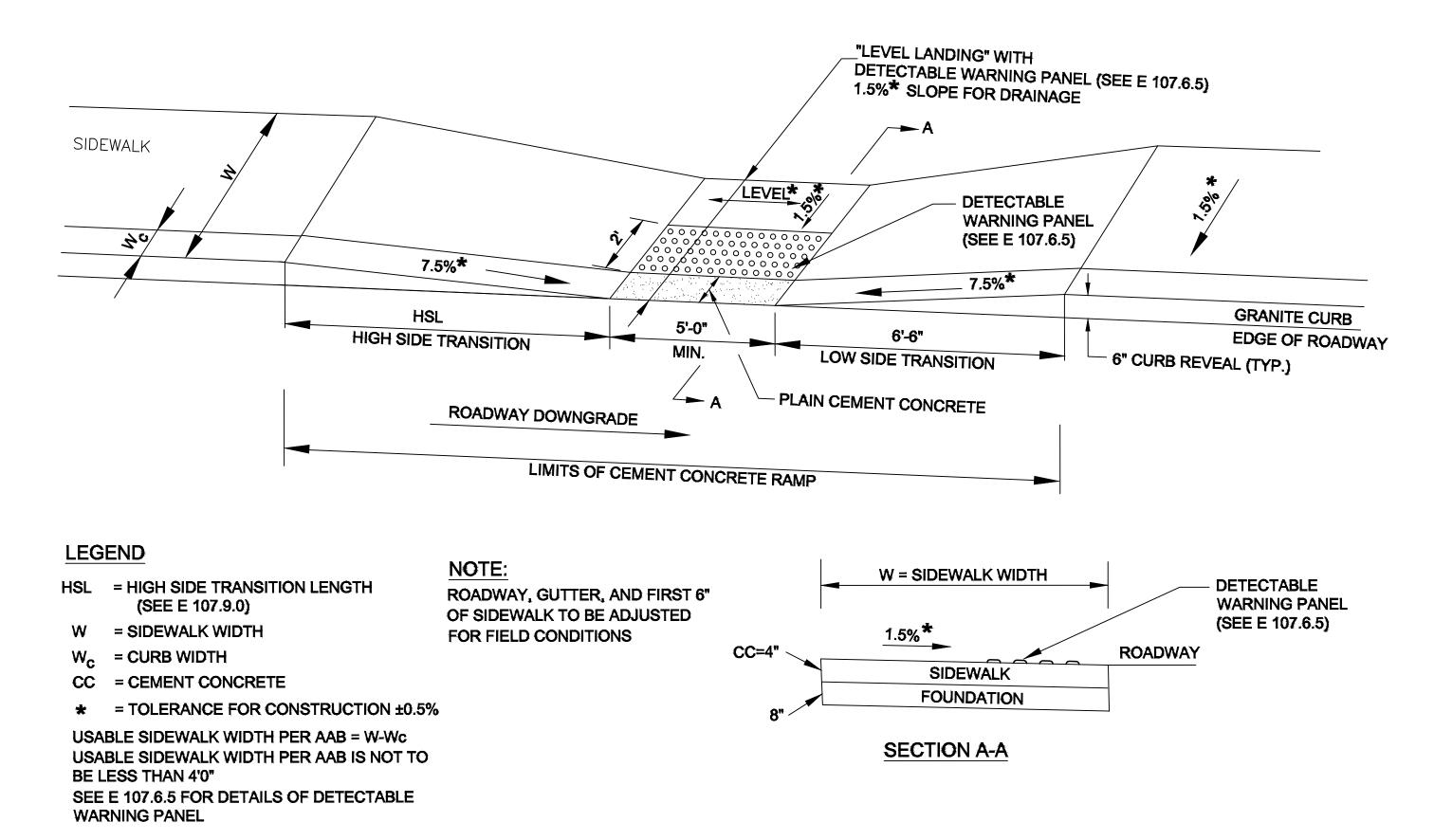
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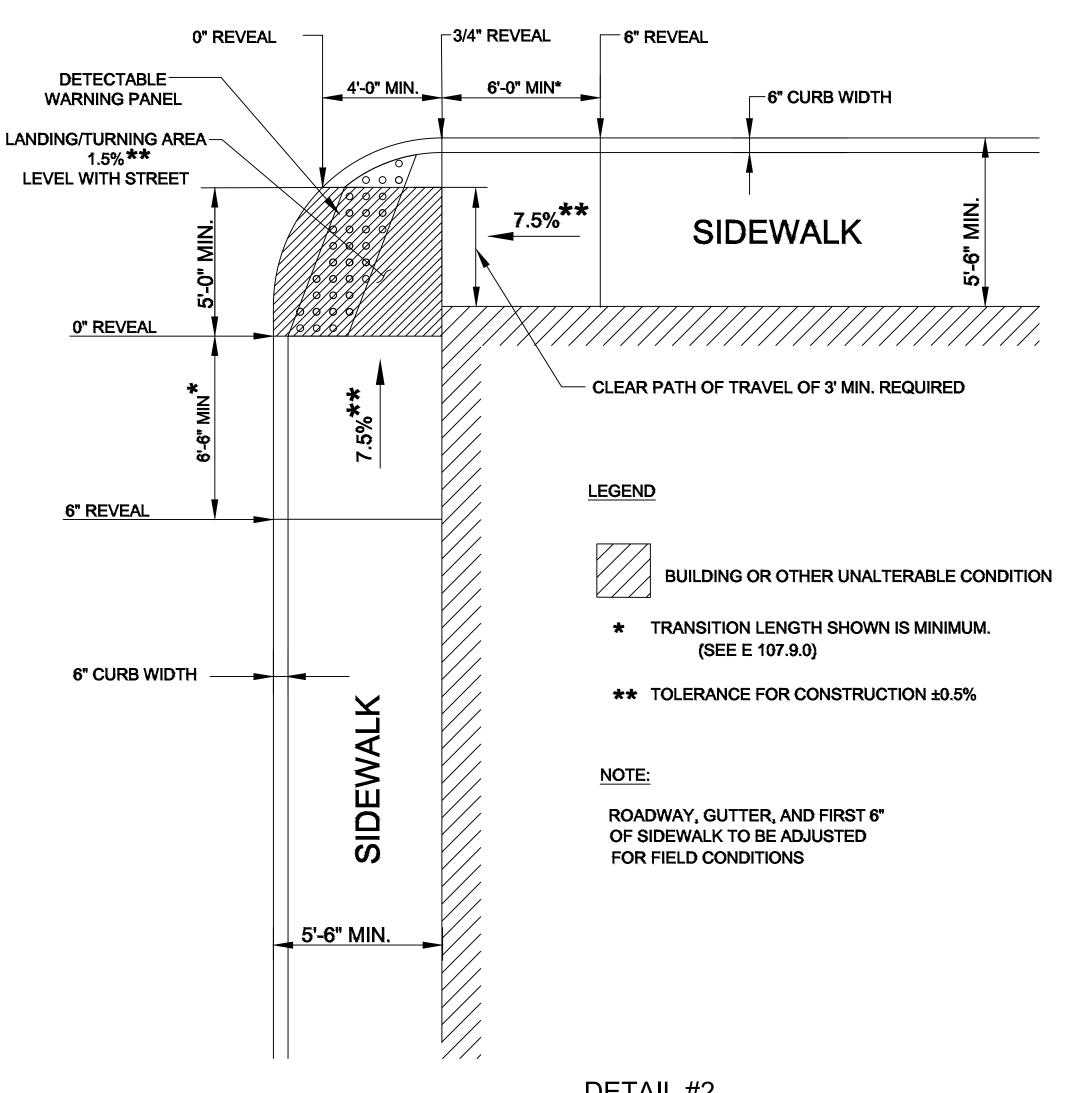
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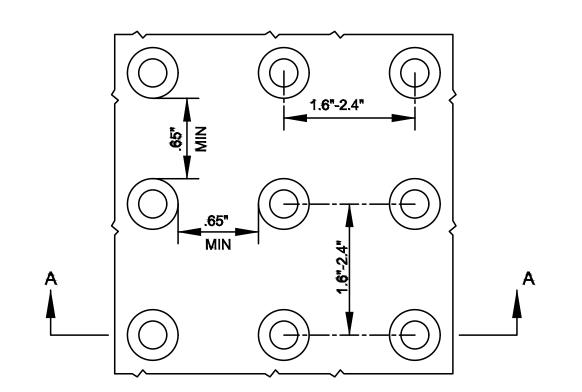
DETAIL #1 WHEELCHAIR RAMP FOR ONE CONTINUOUS DIRECTION OF PEDESTRIAN TRAVEL NOT TO SCALE



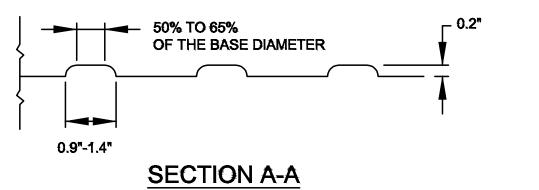
DETAIL #3 WHEELCHAIR RAMP ON NARROW SIDEWALK WITH DETECTABLE WARNING PANEL NOT TO SCALE



DETAIL #2 "T" INTERSECTION WHEELCHAIR RAMP NOT TO SCALE



DETAIL OF DETECTABLE WARNING PANEL



NOTE: PANELS MUST BE CAST IRON PERMANENTLY APPLIED TO THE RAMP. DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH ADJACENT WALKING SURFACES EITHER LIGHT-ON DARK, OR DARK-ON-LIGHT

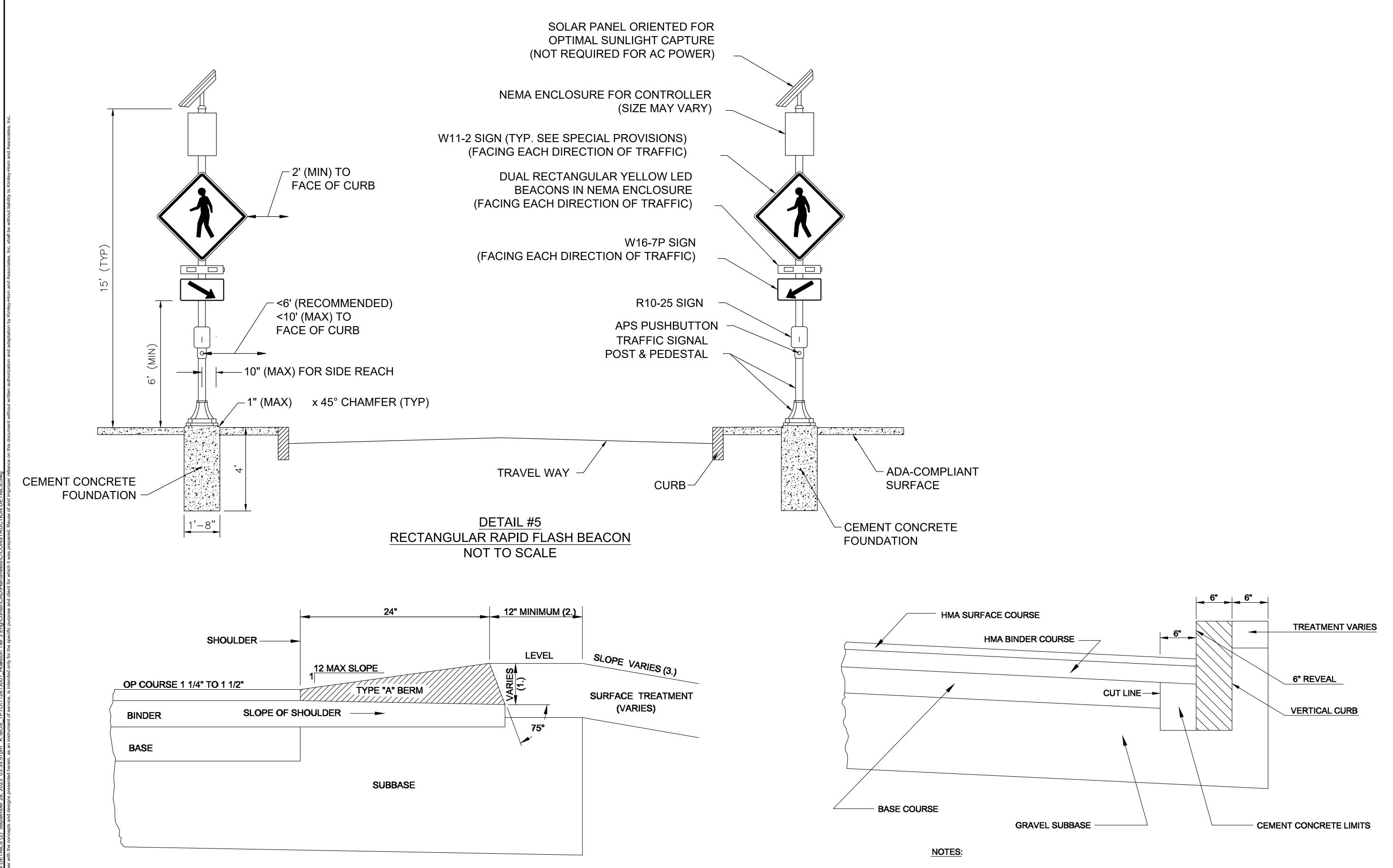
DETAIL #4 DETECTABLE WARNING PANEL FOR WHEELCHAIR RAMPS AND STANDARD RAMP TERMINOLOGY NOT TO SCALE

DETAIL CONSTRUCTION

SEPARED FOR OF HOLLISTON COMPLE

SHEET NUMBER

13



NOTE:

FOR MODIFIED BERM THE SLOPE REMAINS CONSTANT AT 1 (V) TO 12 (H)

2. SEE E 401.1.1 FOR TYPICAL SECTION AT GUARDRAIL LOCATIONS

3. SEE TYPICAL SECTIONS FOR PROJECT

1. THIS DIMENSION VARIES WITH THE THICKNESS OF THE TOP COURSE AND SLOPE OF BINDER

DETAIL #6

CAPE COD BERM

NOT TO SCALE

- 1. THIS PROCEDURE IS APPLICABLE ONLY IF CURB IS TO BE SET AFTER BASE COURSE IS IN PLACE PRIOR TO BINDER AND TOP PLACEMENT.
- 2. CUT NEAT LINE 6" FROM CURB LINE AND REMOVE BASE AND GRAVEL. REPLACE WITH CEMENT CONCRETE.

DETAIL

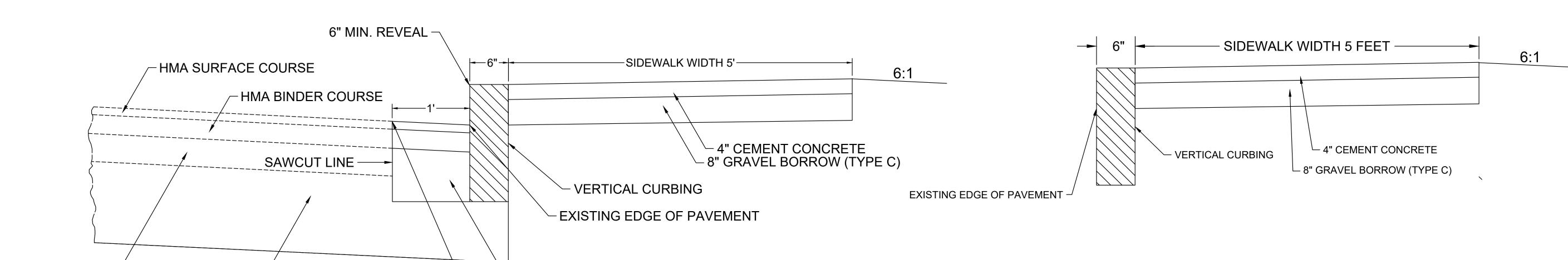
CONSTRUCTION

PREPARED FOR OWN OF HOLLISTON

SHEET NUMBER

3. ANY DESIGNATED CEMENT CONCRETE THAT IS ACCEPTABLE UNDER SECTION M4 OF THE STANDARD SPECIFICATIONS MAY BE USED; ALL TEST REQUIREMENTS ARE WAIVED. HOT MIX ASPHALT SHALL NOT TO BE USED AS A SUBSTITUTE.

DETAIL #7
VERTICAL GRANITE CURBING
NOT TO SCALE



NOTES:

GRAVEL SUBBASE -

BASE COURSE —

EXISTING EDGE OF PAVEMENT -

1. CUT NEAT LINE 1' FROM CURB LINE AND REMOVE BASE AND GRAVEL. REPLACE WITH CEMENT CONCRETE.

CEMENT CONCRETE LIMITS

- MATCH EXISTING ELEVATION

- 2. HOT MIX ASPHALT PATCH TO MATCH EXISTING ROADWAY AND CROSS SLOPES.
- 3. ANY DESIGNATED CEMENT CONCRETE THAT IS ACCEPTABLE UNDER SECTION M4 OF THE STANDARD SPECIFICATION MAY BE USED; ALL TEST REQUIREMENTS ARE WAIVED. HOT MIX ASPHALT SHALL NOT BE USED AS A SUBSTITUTE

DETAIL #8
TYPICAL SIDEWALK SECTION
NOT TO SCALE

- SIDEWALK WIDTH 5 FEET — 6"

4" CEMENT CONCRETE

– 8" GRAVEL BORROW (TYPE C)

DETAIL #9 SIDEWALK CROSS SECTION

AREAS WHERE BACK OF SIDEWALK NEED CURB SUPPORT

NOT TO SCALE

- VERTICAL CURBING

BIT CON
ROADWAY

BIT CON
ADJACENT PROPERTY

6" CEMENT CONCRETE

8" GRAVEL BORROW (TYPE C)

DETAIL #11
SIDEWALK CROSS SECTION
AREAS WHERE FLUSH SIDEWALK TO BE INSTALLED
NOT TO SCALE

DETAIL #10

TYPICAL SIDEWALK CROSS SECTION

AREAS WHERE BACK OF SIDEWALK BLENDS TO

EXISTING GRADE (LOAM OR HANDSCAPE)

NOT TO SCALE

Z W D W

DATE
09/29/2023
SCALE AS SHOWN
DESIGNED BY WJS
DRAWN BY PAG

CONSTRUCTION DETAIL

PROJECTS
PREPARED FOR
WN OF HOLLISTON

COMPLE
PRO
TOWN OF

NOTES:

- ALL TEMPORARY TRAFFIC CONTROL WORK SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND ALL REVISIONS, UNLESS SUPERCEDED BY THESE PLANS.
- 2. ALL SIGN LEGENDS, BORDERS, AND MOUNTING SHALL BE IN ACCORDANCE WITH THE MUTCD.
- 3. TEMPORARY CONSTRUCTION SIGNING AND ALL OTHER TRAFFIC CONTROL DEVICES SHALL BE IN PLACE PRIOR TO THE START OF ANY WORK.
- 4. TEMPORARY CONSTRUCTION SIGNING, BARRICADES, AND ALL OTHER NECESSARY WORK ZONE TRAFFIC CONTROL DEVICES SHALL BE REMOVED FROM THE HIGHWAY OR COVERED WHEN THEY ARE NOT REQUIRED FOR CONTROL OF TRAFFIC.
- 5. SIGNS AND SIGN SUPPORTS LOCATED ON OR NEAR THE TRAVELED WAY, CHANNELIZING DEVICES, BARRIERS, AND CRASH ATTENUATORS MUST PASS THE CRITERIA SET FORTH IN NCHRP REPORT 350, "RECOMMENDED PROCEDURES FOR THE SAFETY PERFORMANCE EVALUATION OF HIGHWAY FEATURES" AND/OR "MANUAL FOR ASSESSING SAFETY HARDWARE" (MASH).
- 6. CONTRACTORS SHALL NOTIFY EACH ABUTTER AT LEAST 24 HOURS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE THE TEMPORARY CLOSURE OF ACCESS, SUCH AS CONDUIT INSTALLATION, EXISTING PAVEMENT EXCAVATION, TEMPORARY DRIVEWAY PAVEMENT PLACEMENT, AND SIMILAR OPERATIONS.
- 7. THE FIRST FIVE PLASTIC DRUMS OF A TAPER SHALL BE MOUNTED WITH TYPE A LIGHTS.
- 8. THE ADVISORY SPEED LIMIT, IF REQUIRED, SHALL BE DETERMINED BY THE ENGINEER.
- 9. DISTANCES ARE A GUIDE AND MAY BE ADJUSTED IN THE FIELD BY THE ENGINEER.
- 10. MAXIMUM SPACING OF TRAFFIC DEVICES IN A TAPER (DRUMS OR CONES) IS EQUAL IN FEET TO THE SPEED LIMIT IN MPH.

WORK VEHICLE

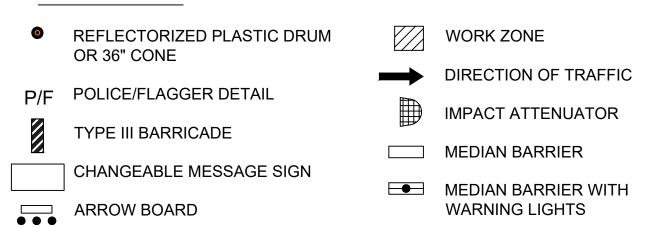
■ SIGN

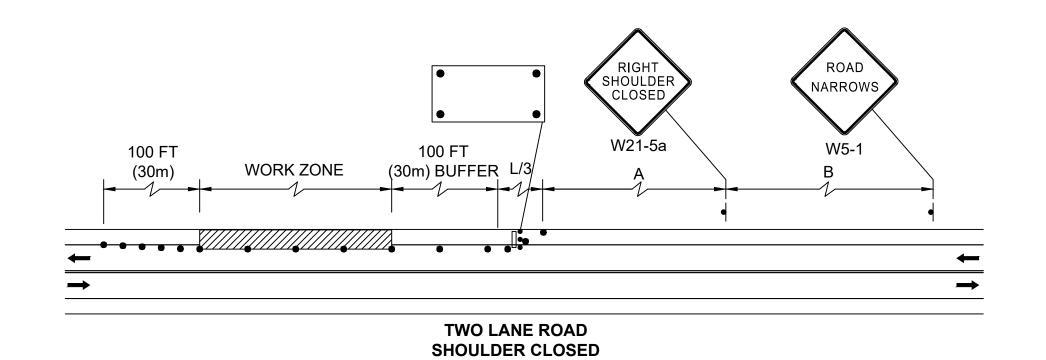
TRUCK MOUNTED ATTENUATOR

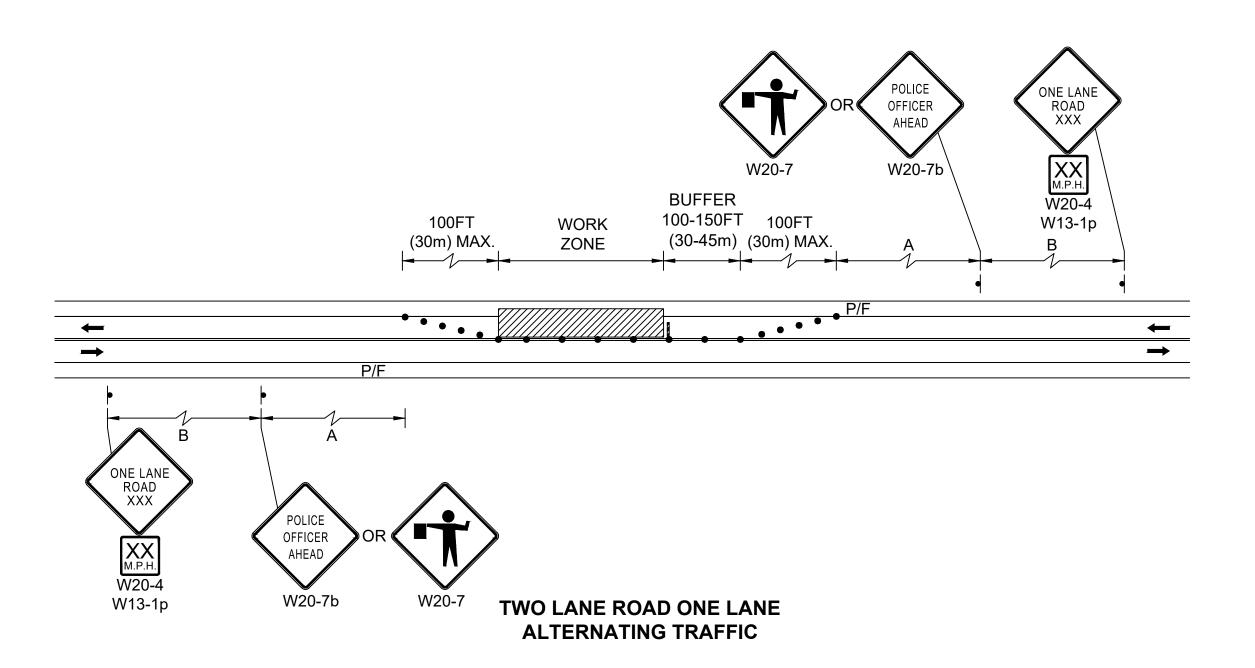
→ TRAFFIC OR PEDESTRIAN SIGNAL

- 11. MINIMUM LANE WIDTH IS TO BE 11 FEET (3.3m) UNLESS OTHERWISE SHOWN. MINIMUM LANE WIDTH TO BE MEASURED FROM THE EDGE OF DRUMS OR MEDIAN BARRIER.
- 12. ALL SIGNS SHALL BE MOUNTED ON THEIR OWN STANDARD SIGN SUPPORTS.

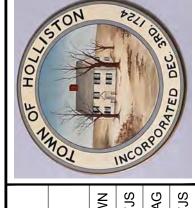
LEGEND:











DATE

09/29/2023

SCALE AS SHOWN
DESIGNED BY W.IS

TRAFFIC MANAGEMENT

PROJECTS
PREPARED FOR
TOWN OF HOLLISTON